Annual Report **2013 | 2014**

ROADS AUTHORITY

SAFE ROADS TO PROSPERITY


Don't worry about your journey,
focus on the destination...

CONTENT

VISION, MISSION AND VALUES

VISION

A sustainable road sector, which is ahead of national and regional socio-economic needs in pursuit of Namibia Vision 2030.

MISSION

Manage a safe and efficient national road network to support economic growth.

VALUES

Innovation
We promote innovative thinking by encouraging staff to keep a constructive and optimistic mindset in pursuit of freedom for creativity, respect for each other, and celebrating diversity so that everyone can give their best.

Service Excellence
We guarantee our stakeholders of our dedicated management of their stakeholders' advantage and the road network, in accordance with the rational principles entrenched in our distinctive legislation.

Open Communication
By adhering to ethical standards and legal requirements in all the operations of the Roads Authority, we shall be honest, fair, open, and equitable in everything we do.

Good Governance
By being honest in all our dealings on behalf of the organisation both internally and externally we actively demonstrate a zero tolerance policy for corruption.

Quality
Performing tasks and assignments right the first time, in accordance with the best practices and standards to compete with the best in the world.

Teamwork
In sharing the Roads Authority's vision, we rely on our team to get results.

Commitment
Being passionate about what we do, we are committed to following through and participating with the highest enthusiasm and energy levels.

BRAND PROMISE
SAFE ROADS TO PROSPERITY

Ms Hileni Kaifanua, Chairperson

Mr Pedro Maritz, Deputy Chairperson

Ms Mariette E. Hanekom, Board Member

Prof. Frank P.L. Kavishe, Board Member

Mr John B. Mukoya, Board Member

BOARD OF DIRECTORS

CHAIRPERSON'S REPORT

The Roads Authority performed outstandingly during the year under review, maintaining the national road network and focusing on efforts to make our roads safer. Work to strengthen the company's market position was intensified and prioritised.

I am pleased to note that we appointed a substantive Chief Executive Officer, Mr Conrad Mutonga Lutombi. He is an accomplished leader that brings a wealth of experience in Leadership and Management as demonstrated by a number of milestones achieved by the Authority. We are confident that Mr Lutombi will steer the organisation towards realising our mandate.

Various operations to improve service delivery to our customers, the road users, were introduced. A Customer Service Charter, which is an important landmark that will guide the organisation in its service delivery, was implemented. In our efforts to improve road safety and ensure that Driving Licences and Roadworthy Certificates are issued with due diligence, Examiners and Supervisors from various Testing Stations and Registering Authorities countrywide completed six months of Refresher and Examiner Training.

A five year blading programme was also implemented, where two main Contractors worked simultaneously across the country. A total of 1 455 kilometres were resealed and 1 244 kilometres were treated with a rejuvenation spray. This resulted in the reduction of expenditure for routine maintenance by 14%.

Projects

Though funding remains a challenge, we continued to record major significant milestones. We completed the upgrading to bitumen standard of Trunk road 15/1: Tsumeb-Katwitwi, Section C in the Kavango Region, construction of the Ovitoto Bridges in the Otjozondjupa region, the upgrading and rehabilitation to dual carriageway of the Okandjengedi and Ongwediva Bridges in Oshana Region, construction of the labour based District Road 3448: Kaisosi to Cuma in the Kavango Region and the construction of a fully-fledged NaTIS Testing Centre at Outapi in the Omusati Region.

Our national road network totals 46 376 kilometres of which 7 165 kilometres are bitumen standard roads. This figure signifies our efforts to expand our road network and to connect to all corridors in the SADC region. The aim being to further promote regional development and integration.

The Roads Authority, as an active partner in the Walvis Bay Corridor Group, continued to support the Group's initiatives along the Trans-Kalahari, Trans-Caprivi and Trans-Cunene Corridors. These corridors promote the drive for trade and economic growth, enhancement of regional co-operation and promotion of tourism which will contribute to making Namibia a transport hub in the near future.

We plan to focus on the future and accelerate the construction of road projects and the maintenance thereof. In light of the challenges we faced, we continued to look to implement new strategies to further enhance the company's performance.

The organisation commenced with the construction of the following capital projects during the reporting period:

- Upgrading to a dual carriageway of the Windhoek-Okahandja Road in the Khomas Region;
- Upgrading to bitumen standard of the Rosh Pinah-Oranjemund road in the !Karas Region;
- The upgrading to bitumen standard of the Aranos-Aminius Road in the Omaheke region;
- Upgrading to bitumen of the Otjinene-Okamatapati-Grootfontein road in the Omaheke and Otjozondjupa Regions respectively; and
- Upgrading to bitumen standard of the Eenhana-Oshigambo road in the Ohangwena region.

Corporate Social Responsibility

As a responsible corporate entity, the Roads Authority sponsored projects aimed at poverty alleviation, HIV/Aids Awareness, job creation, income generation, education and entrepreneurial skills development. Remaining true to our motto to promote SME development, 139 SMEs were contracted on labour-based and road maintenance projects. Approximately 800 jobs were created and 32 SMEs were trained.

A shortage of skills, especially in the engineering field contributed to some of the delays in implementing our projects. Competition for scarce resources in this particular field remains a concern. To build capacity, the Roads Authority awards bursaries to new students on an annual basis and supports a number of students pursuing their studies in the engineering field at tertiary institutions in Namibia.

Acknowledgements

In conclusion, on behalf of the Board, I wish to extend our gratitude and appreciation to Honourable Erkki Nghimtina, Minister of Works and Transport, for his direction and guidance.

I also want to thank the management and employees of the Roads Authority whose efforts helped us achieve so much during the period under review, as we set the foundation for a safe and reliable national road network. In particular, I want to express my appreciation to Mr Lutombi, the Chief Executive Officer, for the leadership that he showed and the inspiration that he has provided to the organisation.

Finally, I would like to thank all my fellow Directors for their contribution towards making sure that the Roads Authority achieved its mandate.

MS HILENI KAIFANUA
Chairperson

CORPORATE GOVERNANCE

The Roads Authority is an organisation that procures all its services and goods through public tendering where it is essential to operate with high ethical standards and transparency. Corporate Governance and the management of risks through Internal Audit among others, will assist Roads Authority in ensuring high ethical standards and transparency.

Board Composition

The Board of Directors are appointed by the Minister of Works & Transport. The Board is comprised of five independent, non-executive directors, including the Chairperson of the Board.

The Roads Authority has a governance structure comprised of the Board of Directors, with three sub-committees that assist the directors in the execution of their mandate. The subcommittees are: the Audit Committee, the Board Tender Committee and the Human Resources Board Committee. Each committee has four scheduled annual meetings; however, the frequency may vary depending on the task at hand. The Board is guided by the Board Charter and its Committees by Terms of Reference, which are renewed annually.

Audit Committee

The Committee consists of Prof. F.P.L. Kavishe as Chairperson, Ms M.E. Hanekom as member and Mr G. Itembu as Co-opted Audit Committee member. The Committee sits at least four times per annum. It is tasked to deal with risk management, internal controls, financial reporting processes, and auditing processes.

The Audit Committee assists the Board in discharging its duties, which are to ensure that adequate internal controls and systems are in place for the reliability of the financial results and accountability for the organisation's assets. The Committee also focuses on anti-corruption, fraud and theft.

Board Tender Committee

The Board Tender Committee is responsible for reviewing tenders, expressions of interest and other procurement in excess of N$20 million upon recommendation from the Management Tender Committee. The Board Tender Committee was mainly established to assist the Board in the execution of its duties in terms of the Tender Rules and Procedures.

The members of the Board Tender Committee are Ms H. Kaifanua, Mr P. Maritz, Prof. F.P.L. Kavishe, Mr J.B. Mukoya and Ms M.E. Hanekom.

Human Resources Committee

The Committee consists of two members: Mr J.B. Mukoya as Chairperson and Ms M.E. Hanekom as member. The Committee meets four times per annum. It is tasked to create an organisational culture, structure and process that supports the development of employees and actualisation of potential performance.

Disclosure of interests

Disclosure of Interest is a standard agenda item at every Board and its Committee's meetings. In the event that there is a conflict of interest, the director must recuse himself from the deliberations relating to that conflict.

Board Evaluation

A Board evaluation is conducted every second year in order to ensure that the Board is effective and executes their duties accordingly.

Mr Conrad M. Lutombi, Chief Executive Officer

Ms Fiina Nkandi, Divisional Manager: Construction and Rehabilitation

Mr Horst Schommarz, Divisional Manager: Maintenance

Mr Melvin van Wyk, Acting Divisional Manager: Transport Information and Regulatory Services

Ms Rauna Hanghuwo, Divisional Manager: Network Planning and Consultation

Ms Rosalinde Nakale, Divisional Manager: Corporate Services

Mr Vincent Sasele, Divisional Manager: Road Traffic and Transport Inspectorate

Ms Sophie Belete-Tekie, Divisional Manager: Road Management System

EXECUTIVE COMMITTEE

CHIEF EXECUTIVE OFFICER'S STATEMENT

I am delighted to present the Roads Authority Annual Report for the 2013/14 fiscal year.

This report, which reflects the successes and challenges of the Authority in the financial year under review, is based on the organisation's Balance Scorecard which responds to the Authority's Strategic Plan linked to NDP4 goals and Vision 2030.

After 14 years since its inception, I am privileged to report that we have gained in confidence to discharge our mandate from the Government and people of Namibia. Central to this increased confidence is our success in expanding, developing and maintaining our national road network, thereby contributing to economic growth and sustainable development.

The Government of the Republic of Namibia has invested heavily in the expansion of the country's roads to create an appropriate network and this very significant investment continues. We are keenly aware of the high level of accountability expected as we deliver on the Government's objectives for good roads that improve the lives of all Namibians through better access to markets, schools, health services and other essential services. The performance index achieved by the Roads Authority based on its strategic objectives and initiatives for the year 2013/14 was 3.16, meaning that the organisation achieved 90% of its agreed strategic performance targets as outlined in our Strategic Plan.

I am pleased with the progress we are making, especially ensuring that our key objectives of growing capacity and capability, especially in the engineering field, are met and that in the long term the Roads Authority will have adequate staff to carry out its main operations with outside expertise being used in a complementary role when required.

I am also proud to report that that we opened Namibia's Technology Transfer Centre (T2 Centre) during the reporting period. The objective of the T2 Centre is to promote a safe, efficient and sustainable transportation network through innovation and the provision and application of new research and technology. The T2 Centre is currently compiling a Road Transportation Sustainability Plan for Namibia and it is in its final stages.

The Roads Authority continued to assist and develop SMEs. To date 54 SME Contractors with a total contract value of approximately N$120 million per annum over a five year period are working in the road grading operations. The Grader Units (GU) development contract model allows for sustainable development by minimising the risk of the SME contractor through continuous contractual support from the side of the development contractor. In total, the organisation allocated N$356 million to SME contractors which amounts to 40 % of the organisation's budget for maintenance of the road network during the period under review.

Regardless of our achievements, the organisation was faced with a few challenges during the period under review. The backlog of road maintenance and rehabilitation works created an extra maintenance burden, which is even further aggravated by the fact that the pace of much needed road rehabilitation work has been dramatically delayed due to severe budgetary constraints. Hence, we want to speedily move towards significantly reducing the huge backlog by securing more funds for road maintenance and rehabilitation to enable us to establish the balance between road preservation and expansion of the road network.

As it is evident, 2013/14 was a year of highs and lows. It takes real teamwork to deliver the successes and just as much teamwork to manage through the challenges. We are fortunate to have a great workforce that is committed to delivering a good service to all who use or rely on our roads. I would like to thank all the employees for the hard work and cooperation shown during the year.

Finally, please allow me to also express my sincere appreciation to the Board of Directors for the collective leadership and guidance during the year.

MR CONRAD M. LUTOMBI
Chief Executive Officer

MAINTENANCE OF THE ROAD NETWORK

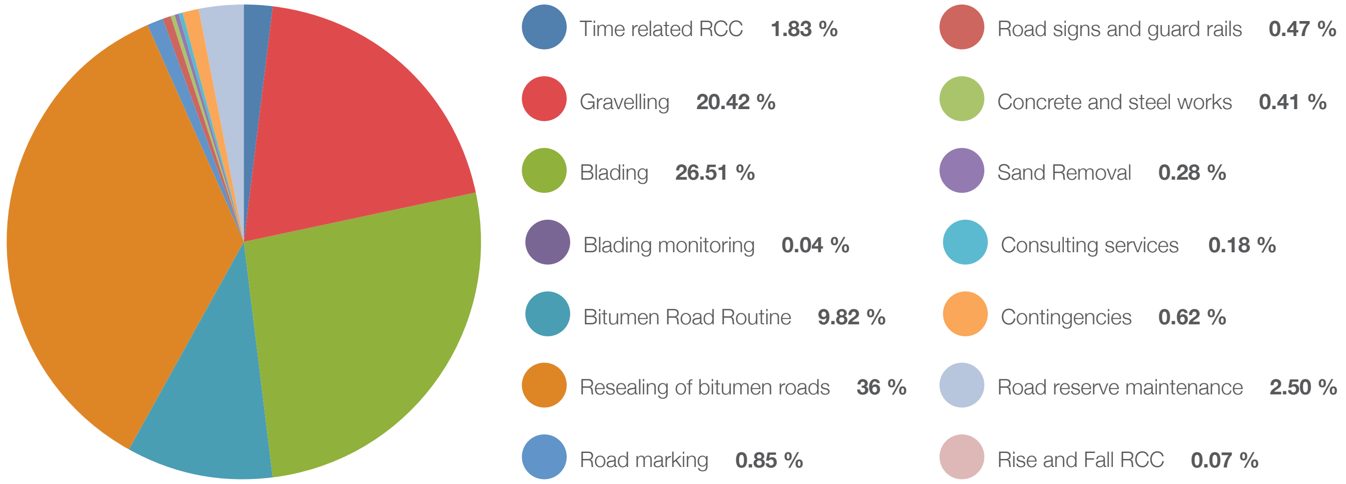
IMPORTANCE OF MAINTENANCE

The Roads Authority carried out the following major operational activities during the period under review:

- Some 1.53 million bladed km were completed within the financial year;
- In total, 691 kilometres of roads were re-graveled. This was achieved with five GRUs (Gravel Re-surface Unit) throughout Namibia. The Roads Authority experienced a severe funding shortage, specifically for re-graveling operations;
- A total of 285 kilometres of salt roads were treated under routine maintenance during the year;
- For routine maintenance of the 6664 km of paved roads, the Roads Authority spent N$91.5 million, equal to N$13,730 per km. The amount spent for routine maintenance could be reduced due to the current reseal project;
- Road markings were done on 865 km of surfaced roads;
- For miscellaneous road maintenance activities, including the maintenance of road reserves, road signs, concrete and steel works for drainage structures, sand removal on TR 4/2 between Lüderitz and Aus, emergency works and special projects such as clearing of drainage facilities with heavy equipment, the Roads Authority spent N$28.5 million; and
- In total, N$923 million (60% of optimum funding requirements according to the Medium- to Long-term Roads Master Plan of 2012, which requires N$1,519 million) was allocated to routine and periodic maintenance. With an expenditure of N$860.6 million, the Roads Authority achieved an expenditure rate of 93.2%.

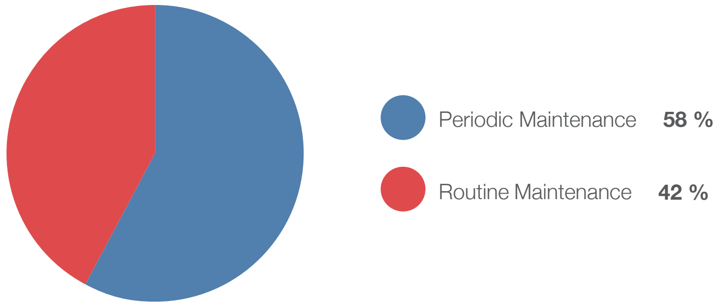
The distribution to different activities is as follows:

Expenditure Distribution: total N$ 891 Million



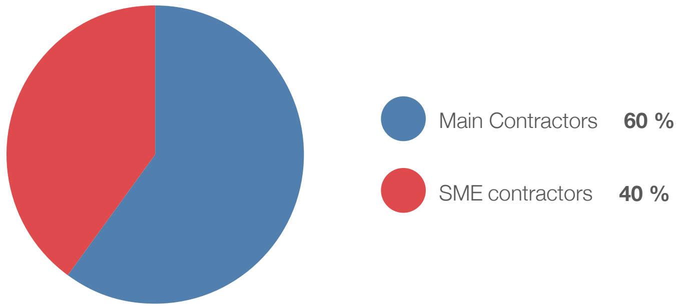
Distribution between Periodic and Routine Maintenance

Distribution Routine and Periodic Maintenance



Participation of SME contractors
A total of N$ 356 million out of N$ 891 million was allocated to SME contractors.

SME participation



THE ROAD MANAGEMENT SYSTEM

ROAD MANAGEMENT SYSTEM

An Integrated Road Management System is an all-encompassing framework including both information processing and human resources for the integrated management of the road network, including the determination and optimisation of economically warranted projects, programmes, strategies and budgets for both development and maintenance.

Road Referencing System (RRS)

The RRS provides the Roads Authority (RA) with one process through which all features, attributes and data may be referenced. It condenses the processes related to road definition and inventory into one system, allowing the entry and updating of information regarding proclamations, road definition, nodes,

road links, lane configuration and cross section details. The RRS also displays information on-line and allows printing of reports for selected roads or areas.

The new network statistics were released and are as depicted in the table. They incorporate all the changes to the network that occurred in 2012 and 2013 and includes new proclamations, upgrades from gravel to bituminous standards, reclassifications of roads and de-proclamations. The existing road network (Network 9) is defined in the Road Referencing System (RRS) and consists of 7165.2 km surfaced roads and 37766.5 km unsealed roads, making a total of 44 931.7 km. Several roads are classified as “Proclaimed only” (1445.9 km), some of which are already maintained as earth roads.

Pavement Management System (PMS)

Road pavements are normally designed to carry the traffic load for 20 years. This means, theoretically, that 5% (358 km) of the total surfaced road length should be rehabilitated (strengthened) per annum. Fortunately, for various reasons, pavement structures in Namibia do last much longer.

Figure 2 shows the distribution of road age according to the 5-year categories explained in the legend.

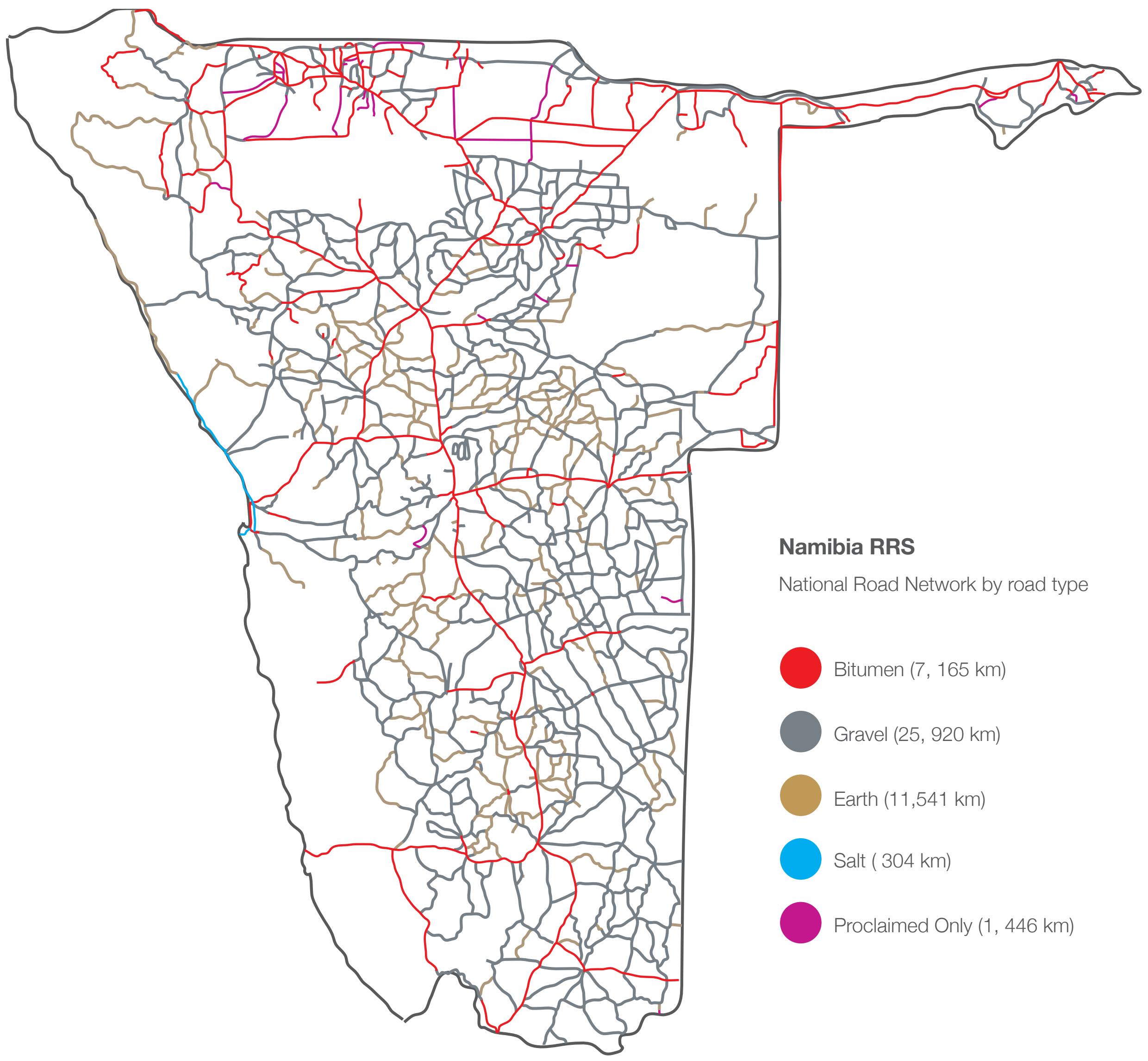


Figure 1: Namibia Road network

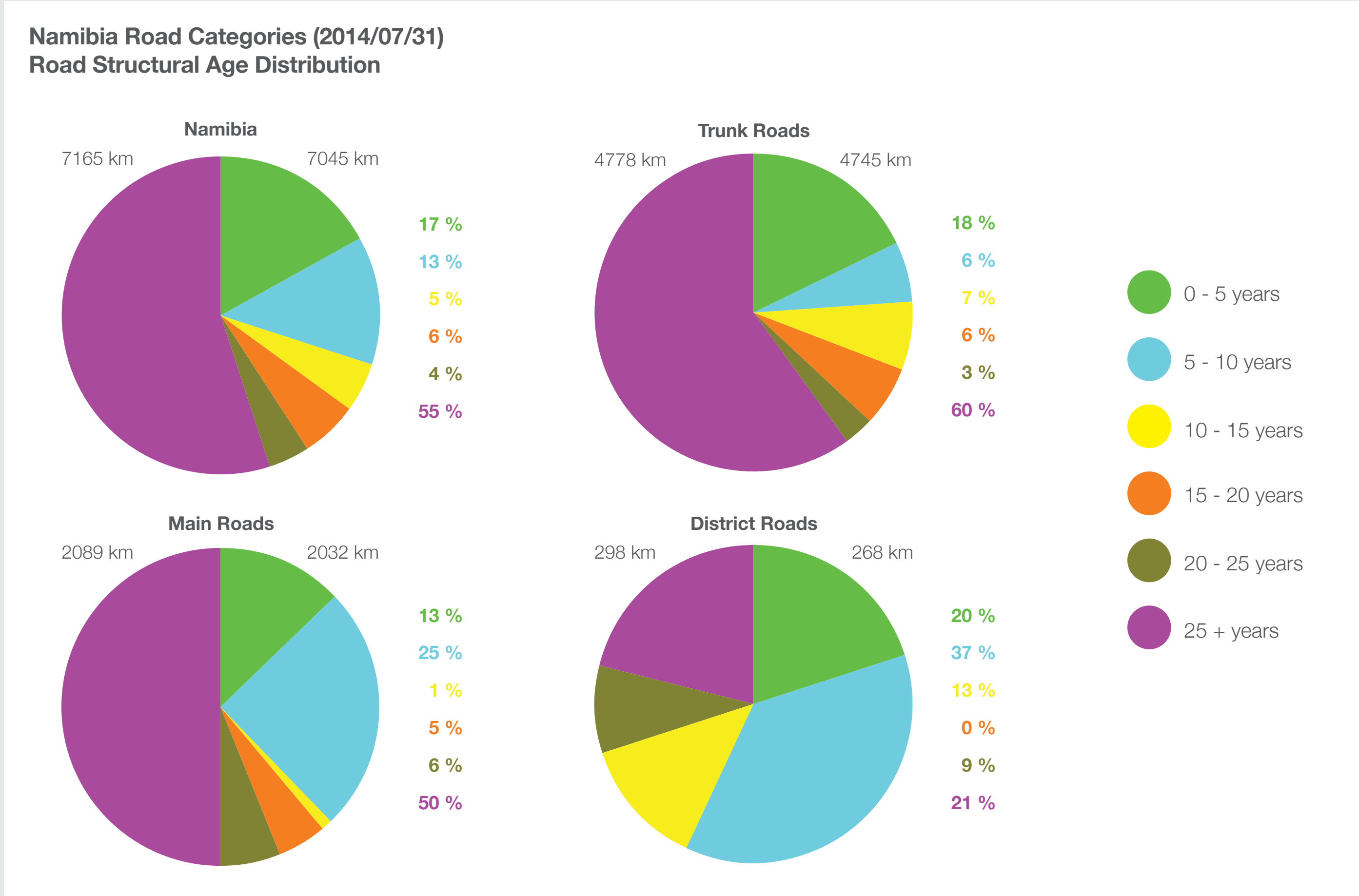


Figure 2: Road Pavement Structural Age Distribution

CURRENT SITUATION:
59 % of our total paved road network is more than 20 years old.

Due to Namibia’s dry conditions, good road building materials and relative light traffic loads, the expected life can be extended with timeous routine maintenance (crack sealing, patching, etc.) and periodic maintenance (resealing).

In 2012, 41% of the bituminous surfacings on our road network were more than 10 years old and 27% older than 15 years. A huge effort was made from 2012 to 2014 in terms of resealing and rejuvenating the higher order roads.

The current situation is as follows:
33% of the bituminous surfacings are older than 10 years and 20% older than 15 years
The figure below shows deterioration/ improvement of the bituminous surfacings.

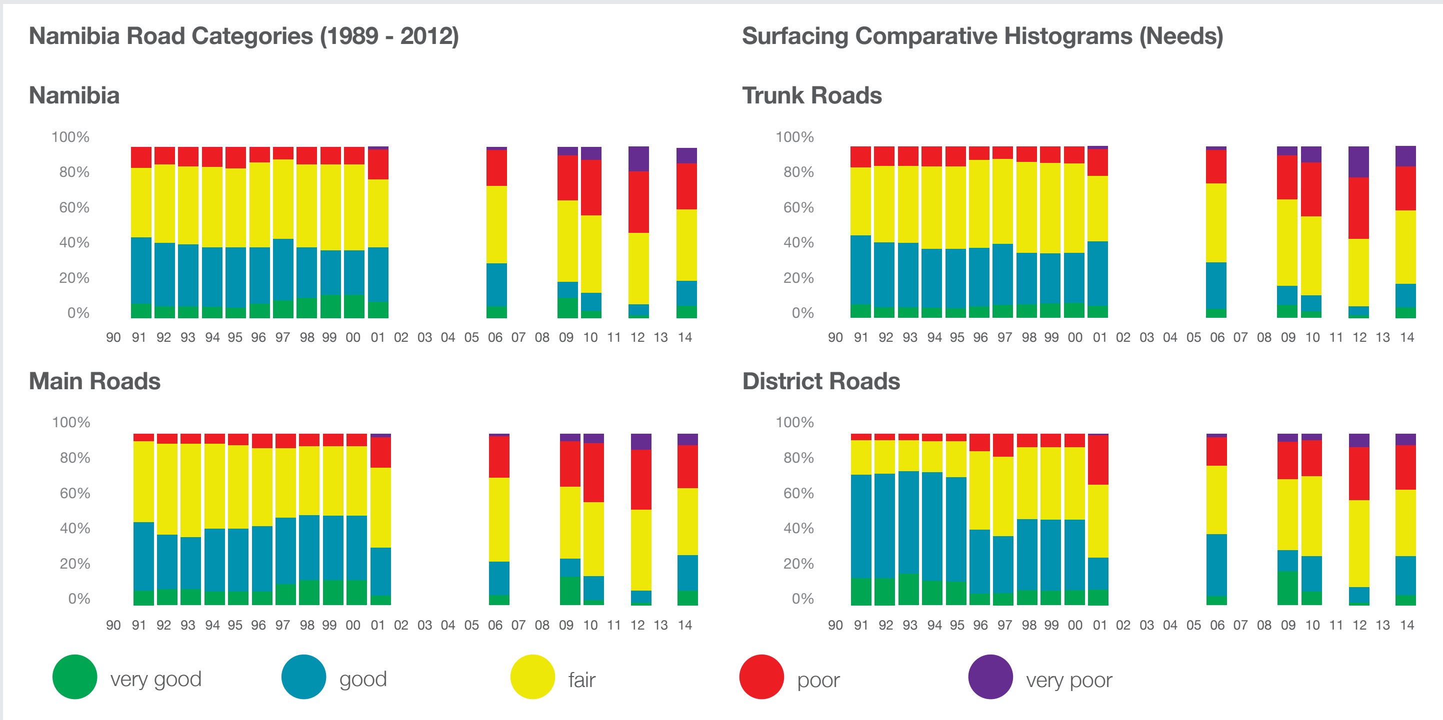


Figure 3: Surfacing Condition (Reseal Need) change over time

The surfaced road network of Namibia deteriorated rapidly from 2000 until 2012 when the RA started with an extensive reseal program. The effort resulted in a significant improvement in the bituminous surfacing condition and temporarily stabilised the deterioration of the pavement structural condition. However, 12% of the road pavements and 11% of the bituminous surfacings are still in a “Poor-to-Very Poor” condition. The bituminous surfacings (seals) typically have an effective life of 10 to 15 years before oxidation results in permeability, allowing water to negatively influence the granular pavement structures. Currently 20% (1433 km) of the bituminous surfacings are older than 15 years and 33% (2364 km) older than 10 years.

Resurfacing (resealing) of a road reduces the ingress of moisture, resulting in a lower rate of deterioration in the pavement structure, and therefore, an extended life of the pavement structure before rehabilitation would be required.

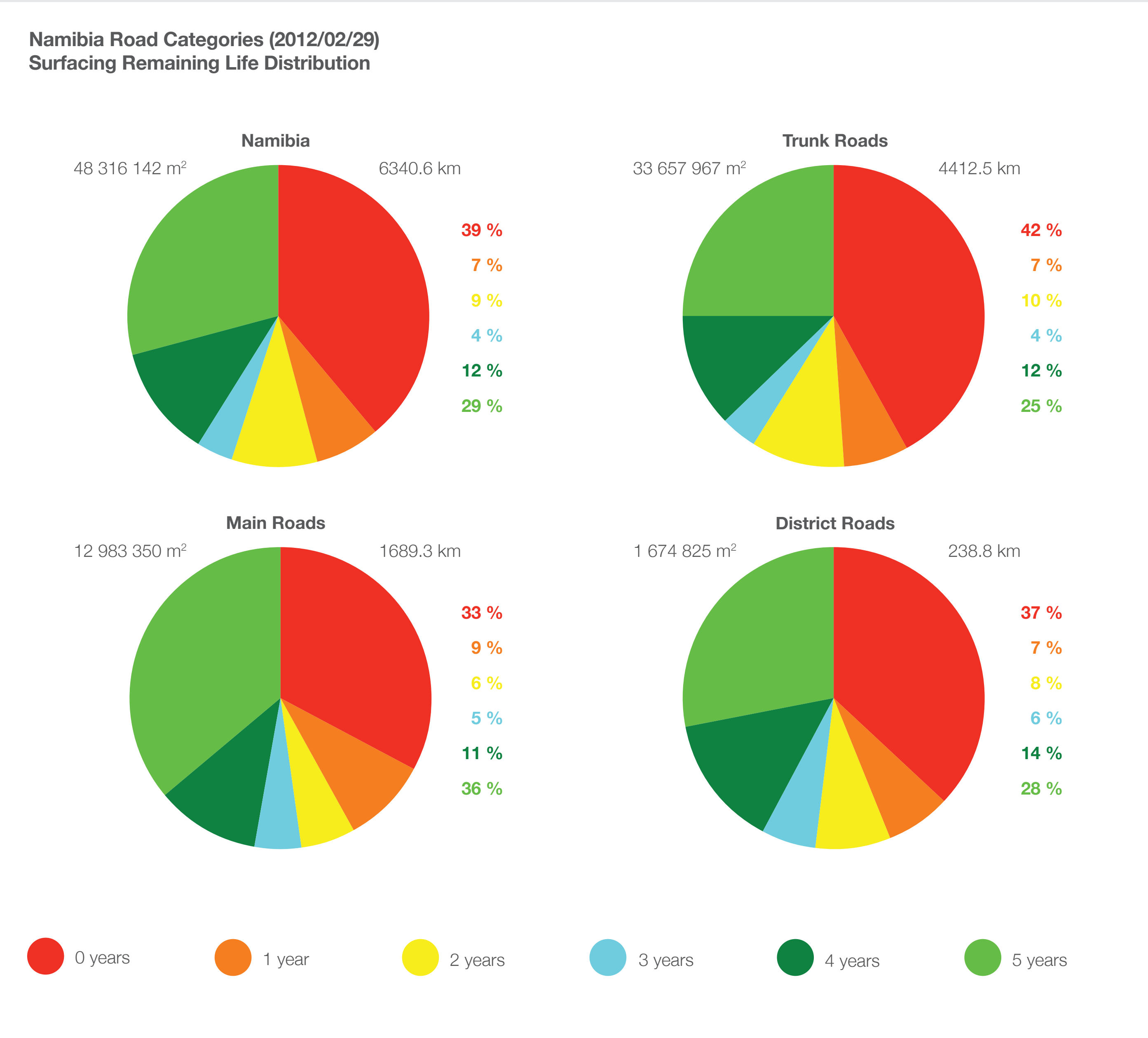


Figure 4: Surfacing Remaining Life Distribution

A conservative calculation indicates a value of approximately N$ 38 504 million to replace only the top layers and bituminous surfacing of our paved roads - therefore, the typical cost to rehabilitate the entire surfaced road network. This does not include the value of the land, earth works, bridge structures, road furniture or the value of unsealed roads.

Unsealed Road Management System (URMS)

The URMS has been developed as part of the Integrated Road Management System of the Roads Authority and serves the purpose of assisting the RA in strategic and tactical planning on the unsealed road network.

Activities required to provide and to maintain a safe and economical unsealed road network are categorised into:

- Routine road maintenance in the form of blading, drainage and road reserve maintenance;
- Scheduled or periodic maintenance to improve accessibility, safety and maintainability e.g. re-gravelling, forming, reshaping;
- Sealing of gravel roads to preserve materials and to reduce maintenance and user costs; and
- Ancillary works which include drainage improvements, emergency works and other maintenance requirements within the road reserve e.g. vegetation control, road signs and fences

The RMS provides graphical displays regarding the change in condition over time. The figure below shows the change in general condition from 2002 to 2012.

Figure 5: Change in visual condition

The general condition of the unsealed road network has deteriorated significantly over the past ten years.

According to the Unsealed Road Management System preliminary findings of 2012/2013 visual assessment survey, 58% of the unsealed road network was classified as being in a “Poor” or “Very Poor” condition with an average thickness of imported gravel of 45mm. (Note: A typical new thickness of the wearing course is 150mm). This indicates that the network condition is deteriorating further comparing to the results of 2010 where only 38% was classified to be in a “Poor” or “Very Poor” condition.

Bridge Management System (BMS)

During the last inspections a total of 1430 structures, classified as bridges and major culverts, were identified.

The majority of the structures were constructed during the period 1960 to 1980. The average age of bridges is 36 years, while the average age of major culverts and large culverts is 32 years.

The majority of structures were constructed in the Hardap and Karas regions. Figure 6 below shows the distribution of different structure types throughout Namibia.

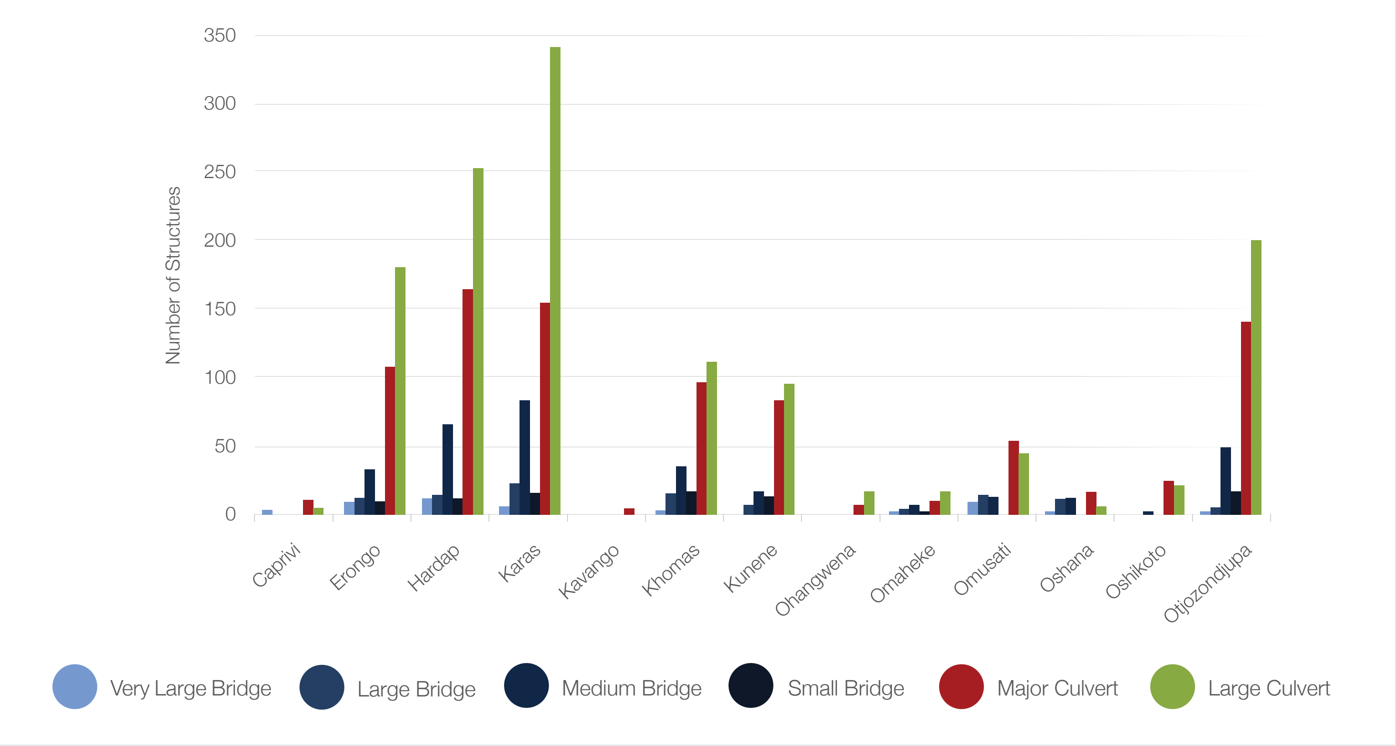


Figure 6: Distribution of structures

Traffic Surveillance System

The Road Management System collects, summarises and interprets information on the traffic traveling the national road network of Namibia through its Traffic Surveillance System. The information is used to assess transportation needs, and to develop programming recommendations. Traffic data is also vital for network planning, design, construction and maintenance of roads. Besides internal stakeholders of RA, TSS is serving consultants, researchers, SMEs, the media and other government agencies. TSS is the first call of port for all Consultants and Engineers involved in road projects. Currently TSS operates 150 permanent and 200 Ad-hoc electronic traffic monitoring sites country wide.

Interesting results from the TSS:

- Heavy Vehicles constituted about 20% of the traffic on the network in 2013.
- The percentage of vehicles that travel at a speed more than 120km/hr has shown consistent decline from 26% in 2004 to 17% in 2013.

The vehicle kilometer travelled (VKT) indicator is used as a common measure of road use. Figure 7 shows the VKT per annum on different road surface types in Namibia.

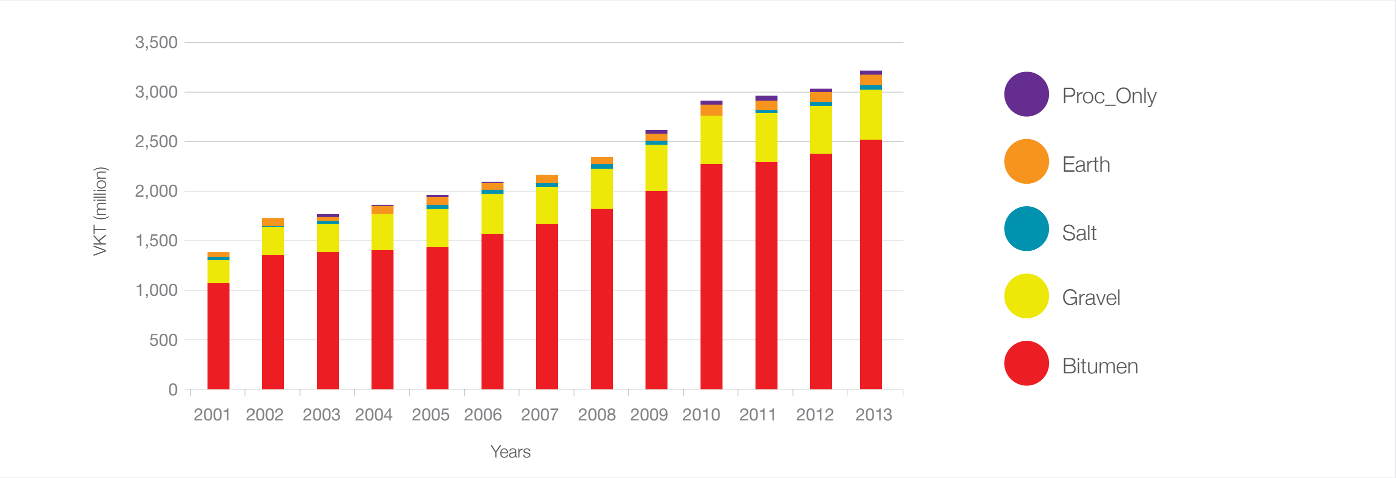


Figure 7: Vehicle Kilometer Travelled (VKT) per Annum

The daily distribution of traffic is shown in Figure 8.

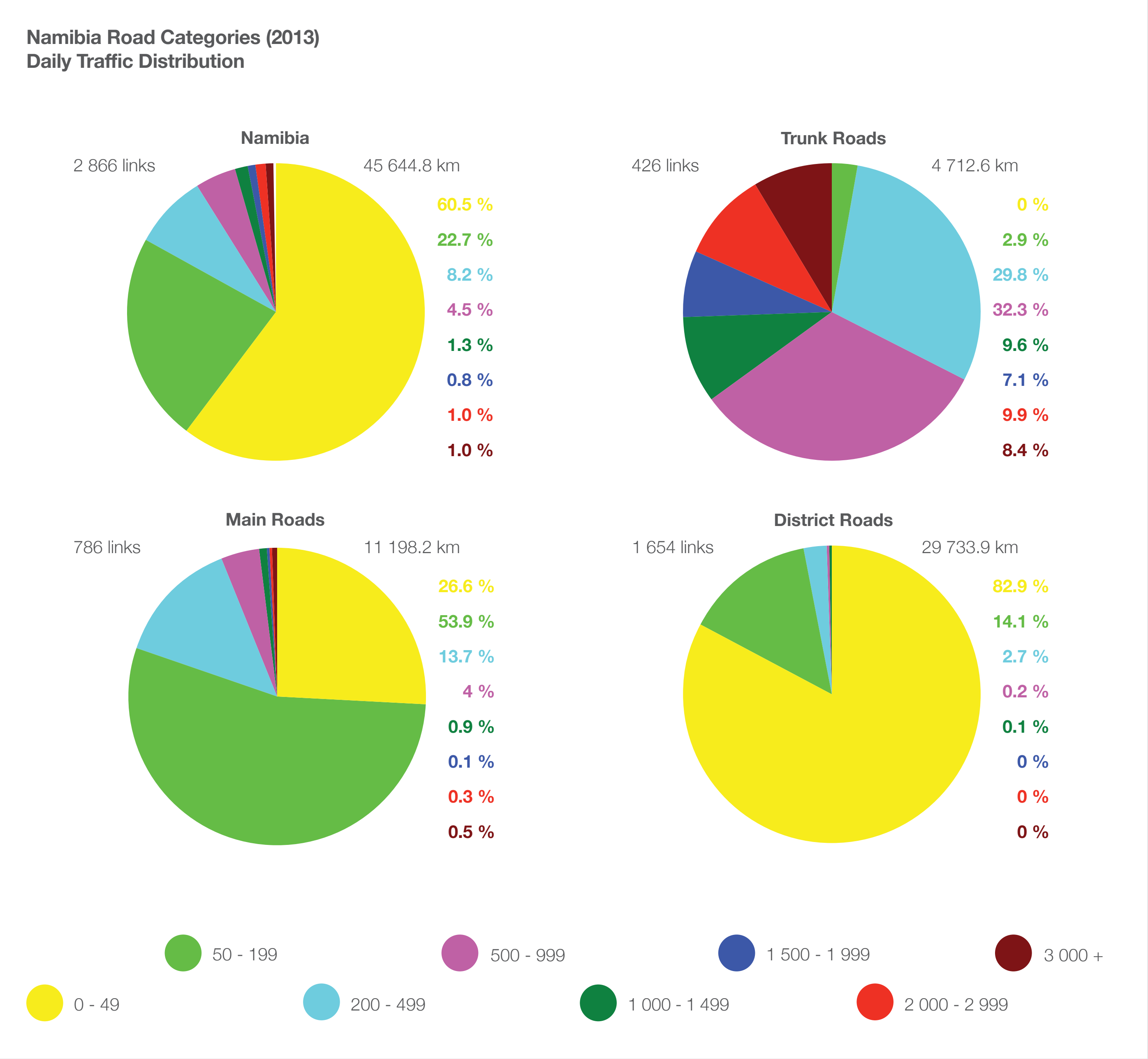


Figure 8: Daily Distribution of Traffic

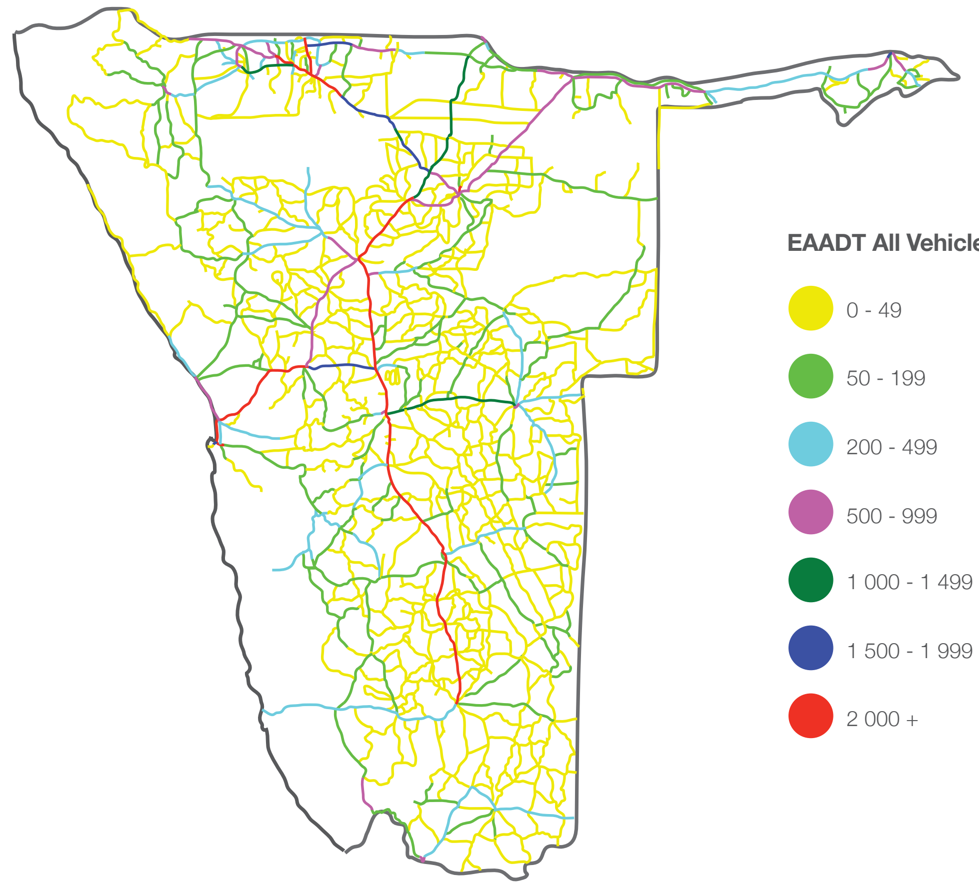
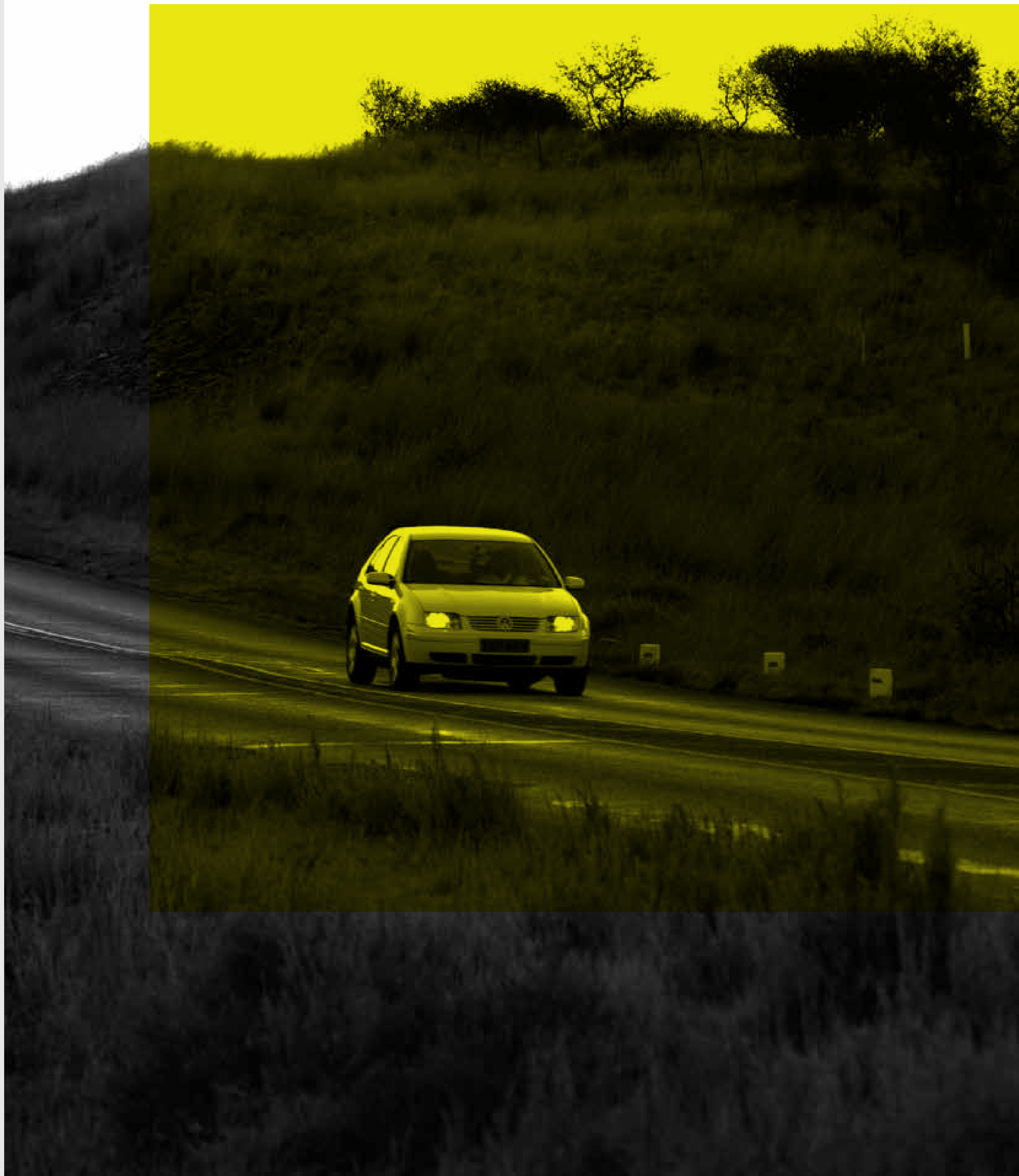
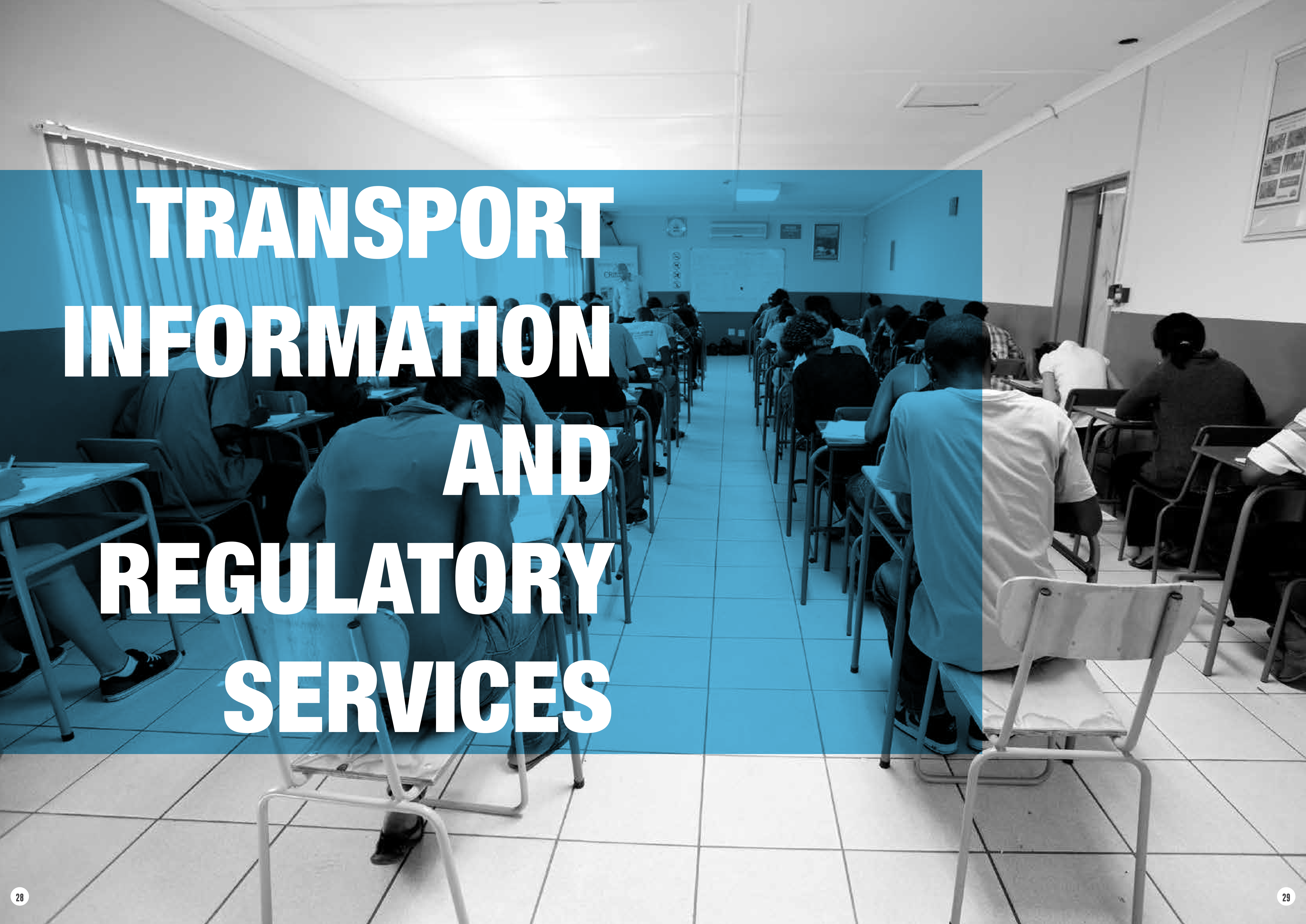


Figure 9 Estimated Average Annual Daily Traffic (2013)

About 2% of the entire national road network accommodates more than 2000 vehicles per day while 60% carries less than 50 vehicles per day.





TRANSPORT INFORMATION AND REGULATORY SERVICES

SADC CREDIT CARD FORMAT DRIVING LICENCE

The Roads Authority advertised a tender to appoint a new driving licence card production contractor on a Built-Operate-Transfer principle whereby the Roads Authority willl take over the production of driving licences at the end of the Contract. In view of the above, the Roads Authority will construct the new Card Production Facility as part of the new Khomasdal NaTIS One-Stop Centre.

The new driving licence card production contract includes both facial and fingerprint biometric technology to curb identity fraud at the NaTIS Offices during the driving licence issuance process. Biometric technology is used as part of a comprehensive Customer Tracking solution to ensure that a driving licence is issued to the same person who successfully completed the learner licence and driving skill tests.

The new driving licence card production contract will also introduce a new driving licence card format and design to further improve the security and integrity features of the current driving licence card.

VEHICLE AND DRIVER TESTING FACILITIES

The construction of a new A-Grade Vehicle and Driver Testing Facility in Outapi was completed and the facility was officially inaugurated on 20 November 2013.

The construction of a new A-Grade Vehicle and Driver Testing Facility in Opuwo started and is scheduled for June 2015.

Preventative maintenance, repairs and calibration on vehicle testing equipment are conducted bi-annually, and calibration certificates are issued by the VTS equipment supplier. During the period under review, The Roads Authority also planned to introduce latest testing equipment at all Vehicle Testing Stations, such as CCTV monitoring, Bluetooth headlight beam testers and a Roadworthy Test result database.

CUSTOMER SERVICE AND BUSINESS PROCESS ENHANCEMENT

The following interventions were introduced to improve our customer service and business processes:

- An annual Customer Communication Plan was implemented to communicate our services to the customers through various communication mediums such as radio, print media and television. We also launched the Roads Authority Road Show under the theme " Know the right thing. Do the right thing." directly with customers and to take NaTIS services to the people. Accordingly, road shows were conducted in the Northern, Western and Southern Regions during the period under review.
- Contracts were signed between service providers for the supply and implementation of the Call Centre Management System and the provision of SMS services to allow customers countrywide to make enquiries at one central point or to receive and send SMSes on operations and statuses of their motor vehicle and driving licences.
- The mapping of all the business processes and documented operating procedures to streamline and standardise operations in terms of vehicle and driver testing, registration and licensing as well as the issuing of domestic and cross border passenger and transport permits was completed during the period under review.

DRIVER POPULATION

Namibia's driver population increased by 9,681, bringing the total population to 245,150, representing an increase of 4.11% compared to the population of the previous year 2012/2013. The following table details the number of drivers per Authority.

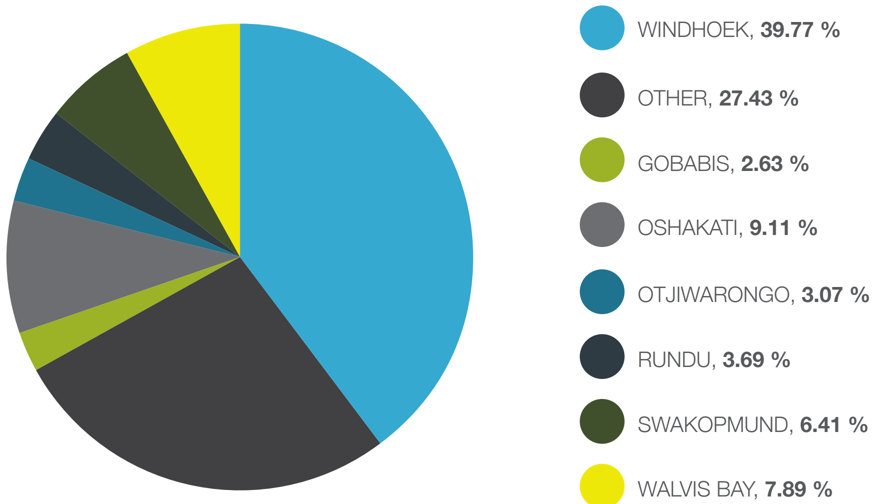
All Authorities	2009/10	2010/11	2011/12	2012/13	2013/14	Distribution	% Growth
ARANOS	386	383	399	464	470	0.19%	1.29%
BETHANIE	183	172	183	207	188	0.08%	-9.18%
EENHANA	1,802	1,896	2,285	3,270	4,113	1.68%	25.78%
GOBABIS	5,204	4,965	5,142	6,222	6,446	2.63%	3.60%
GROOTFONTEIN	3,727	4,028	4,170	4,946	5,344	2.18%	8.05%
KARASBURG	1,471	1,486	1,488	1,712	1,745	0.71%	1.93%
KARIBIB	1,869	1,982	2,130	2,607	2,518	1.03%	-3.41%
KATIMA MULILO	2,670	2,647	2,819	3,488	3,603	1.47%	3.30%
KEETMANSHOOP	4,399	4,390	4,615	5,637	6,170	2.52%	9.46%
KHORIXAS	302	285	320	402	443	0.18%	10.20%
LÜDERITZ	2,324	2,191	2,190	2,555	2,642	1.08%	3.41%
MALTAHÖHE	182	168	169	221	218	0.09%	-1.36%
MARIENTAL	3,878	3,767	3,858	4,593	4,665	1.90%	1.57%
OKAHANDJA	4,916	4,778	4,849	6,098	6,407	2.61%	5.07%
OKAKARARA	253	242	291	385	385	0.16%	0.00%
OMARURU	663	698	755	988	1,053	0.43%	6.58%
ONDANGWA	2,755	2,900	3,209	3,821	3,980	1.62%	4.16%
OPUWO	1,155	1,321	1,536	1,885	2,069	0.84%	9.76%
ORANJEMUND	2,053	1,896	1,812	2,400	2,492	1.02%	3.83%
OSHAKATI	17,068	17,929	19,123	21,901	22,345	9.11%	2.03%
OTAVI	401	381	404	485	450	0.18%	-7.22%
OTJINENE	106	92	113	150	175	0.07%	16.67%
OTJIWARONGO	5,304	5,361	5,754	7,033	7,537	3.07%	7.17%
OUTAPI	2,846	2,788	3,234	4,503	5,245	2.14%	16.48%
OUTJO	2,826	3,032	3,306	3,880	3,789	1.55%	-2.35%
REHOBOTH	1,766	1,725	1,829	2,195	2,349	0.96%	7.02%
RUNDU	5,503	5,900	6,699	8,624	9,050	3.69%	4.94%
SWAKOPMUND	11,422	11,597	12,169	14,924	15,702	6.41%	5.21%
TSUMEB	4,803	4,807	4,995	5,993	6,344	2.59%	5.86%
USAKOS	265	255	280	358	357	0.15%	-0.28%
WALVIS BAY	14,237	15,422	16,598	19,178	19,353	7.89%	0.91%
WINDHOEK	76,826	78,436	81,396	94,344	97,503	39.77%	3.35%
TOTAL	**183,565**	**187,920**	**198,120**	**235,469**	**245,150**	**100.00%**	**4.11%**

GROWTH	2009/10	2010/11	2011/12	2012/13	2013/14
Growth in numbers	6,043	4,355	10,200	37,349	9,681
Growth in [%]	3.40%	2.37%	5.43%	18.85%	4.11%

The increase in the number of drivers normalised after the introduction of the new driving test manual as well as the introduction of a written learner licence exam papers.

DRIVERS POPULATION GROWTH

DRIVERS POPULATION DISTRIBUTION
End of 2014 - Q1

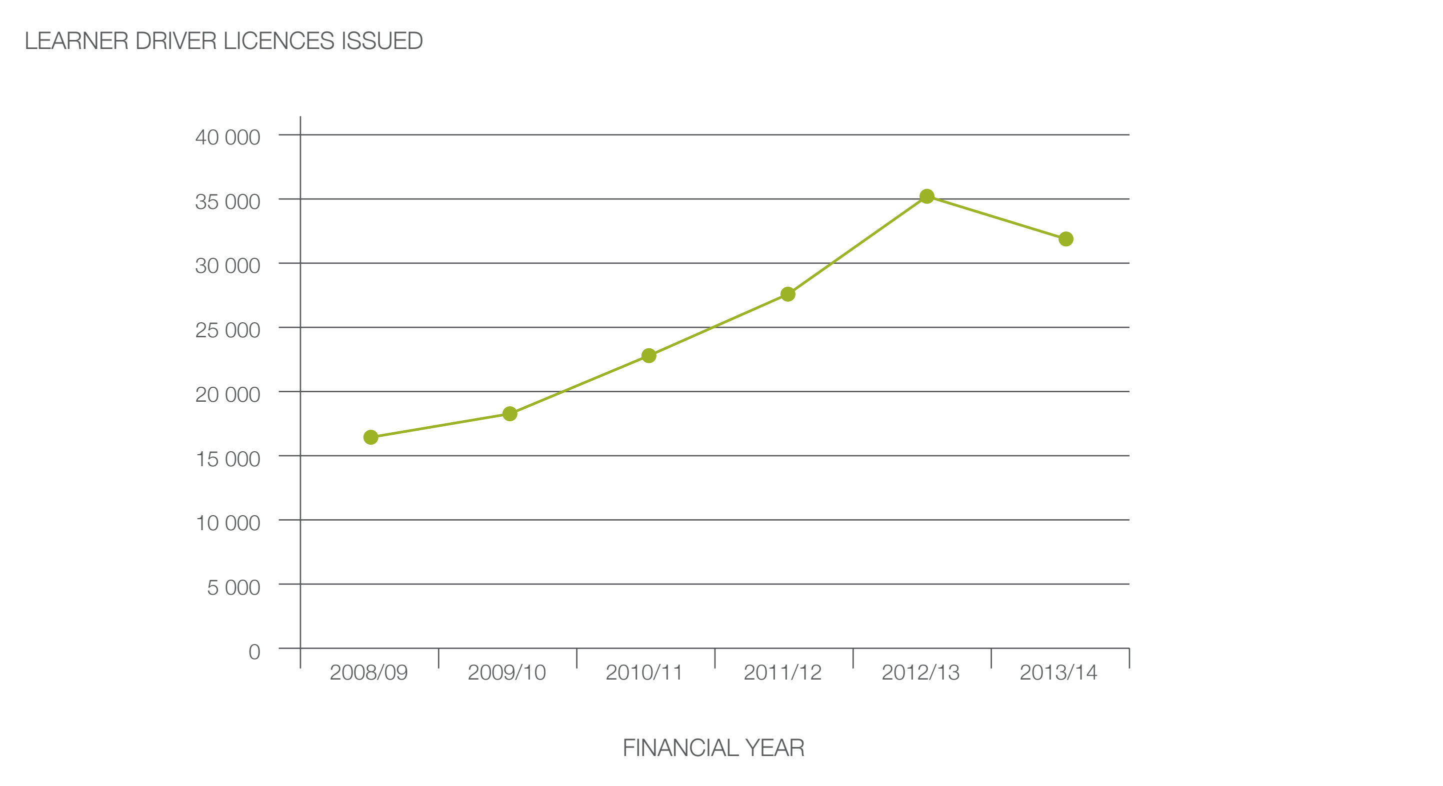


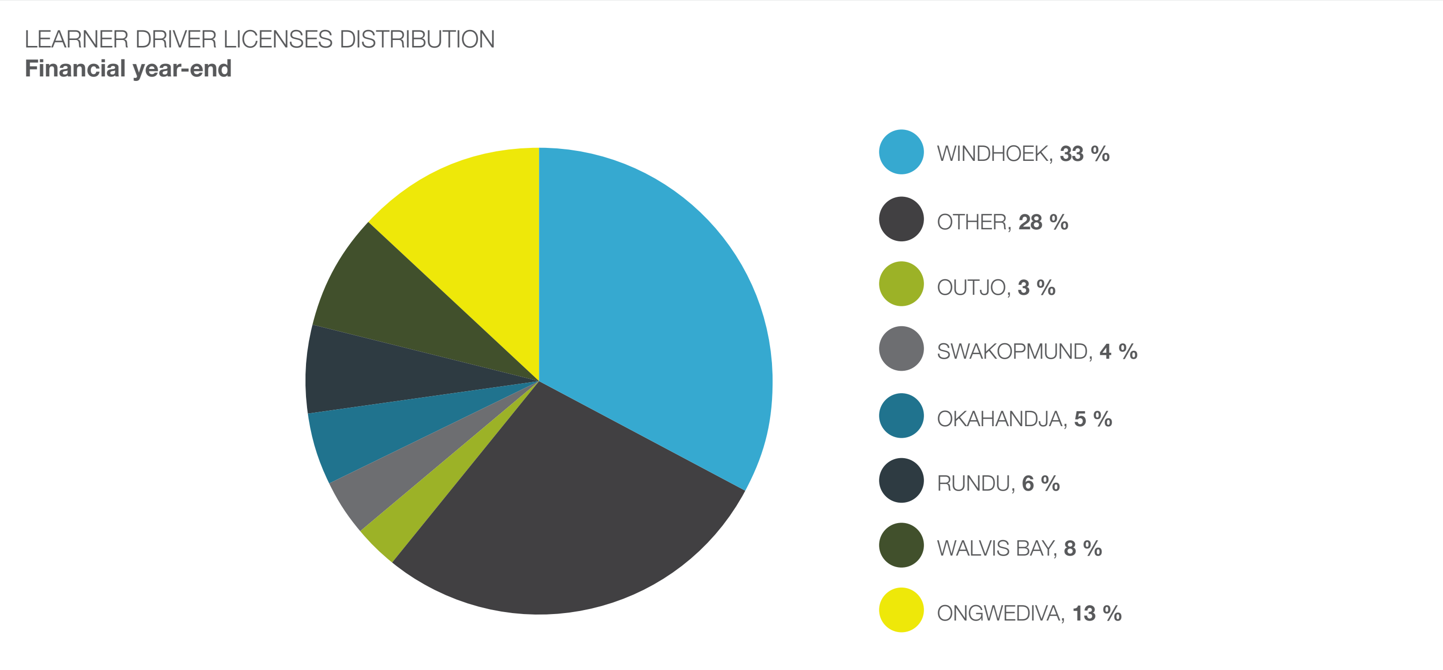
LEARNER DRIVER LICENCES (ISSUED)

Namibia's learner driver licences decreased by 3,451, bringing the total number of learner licences issued to 32,346, representing a decrease of 9.64% compared to the licences issued during the previous year 2012/2013.

Issuing Authority	2009/10	2010/11	2011/12	2012/13	2013/14	Distribution	% Growth
EENHANA	0	681	1,116	846	1,048	3.24%	23.88%
GOBABIS	644	741	898	940	815	2.52%	-13.30%
GROOTFONTEIN	935	1,464	1,200	804	851	2.63%	5.85%
KARASBURG	240	180	124	252	414	1.28%	64.29%
KARIBIB	759	1,163	916	529	498	1.54%	-5.86%
KATIMA MULILO	379	462	614	604	729	2.25%	20.70%
KEETMANSHOOP	379	841	805	506	348	1.08%	-31.23%
LÜDERITZ	217	173	181	276	595	1.84%	115.58%
MARIENTAL	394	464	475	788	628	1.94%	-20.30%
OKAHANDJA	940	1,004	1,322	1,895	1,706	5.27%	-9.97%
OPUWO	579	480	642	504	256	0.79%	-49.21%
ORANJEMUND	108	93	131	249	136	0.42%	-45.38%
ONGWEDIVA	3,308	2,509	2,131	2,666	4,117	12.73%	54.43%
OTJIWARONGO	443	775	926	654	600	1.85%	-8.26%
OUTAPI	0	591	662	811	1,070	3.31%	31.94%
OUTJO	700	890	1,135	792	1,078	3.33%	36.11%
REHOBOTH	0	0	0	0	137	0.42%	NA
RUNDU	1,147	1,323	1,748	1,252	1,786	5.52%	42.65%
SWAKOPMUND	1,034	1,051	1,419	1,565	1,303	4.03%	-16.74%
TSUMEB	656	850	612	718	779	2.41%	8.50%
WALVIS BAY	2,655	4,017	4,066	3,589	2,752	8.51%	-23.32%
WINDHOEK	3,028	3,097	7,247	15,557	10,700	33.08%	-31.22%
TOTAL	**18,545**	**22,849**	**28,370**	**35,797**	**32,346**	**100.00%**	**-9.64%**

GROWTH	2009/10	2010/11	2011/12	2012/13	2013/14
Growth in numbers	1,608	4,304	5,521	7,427	-3,451
Growth in [%]	9.49%	23.21%	24.16%	26.18%	-9.64%



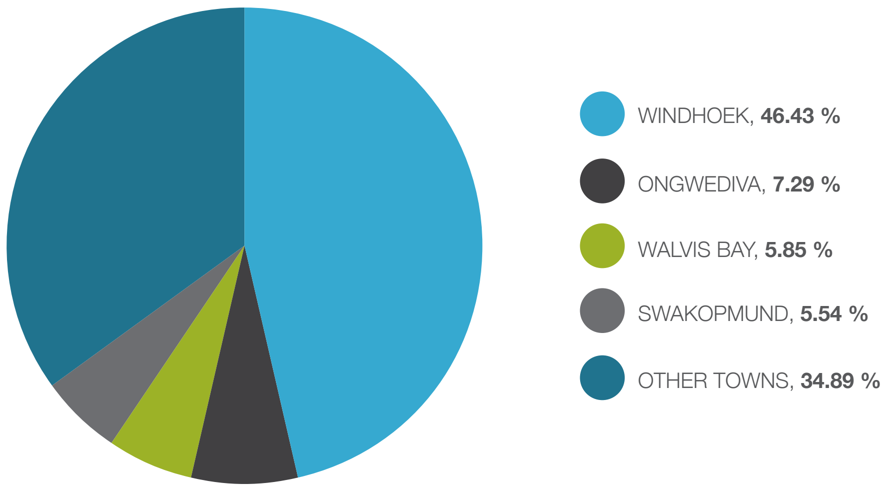


VEHICLE POPULATION

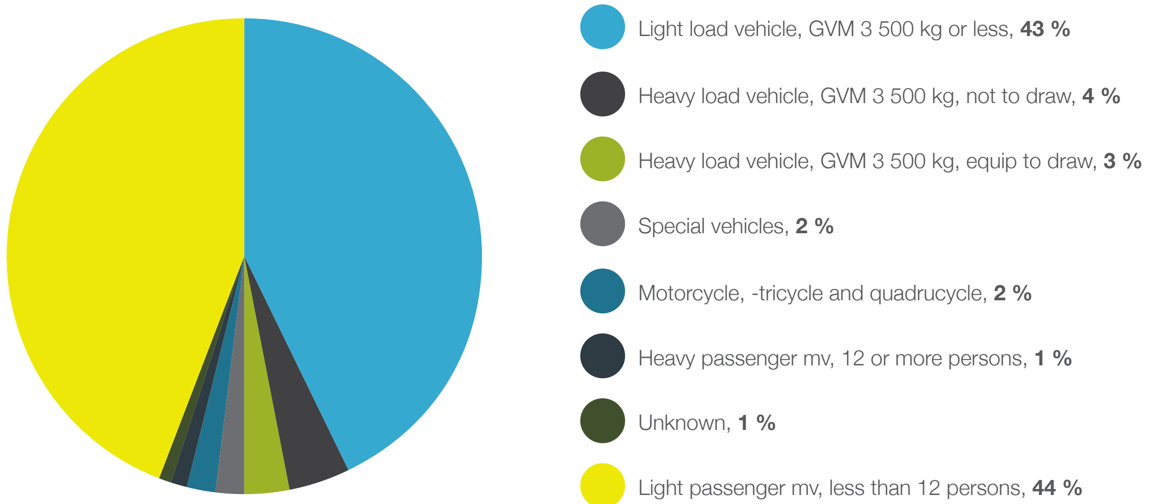
Namibia's registered vehicle population increased by 20,409 (7.13%) to 306,701. The following table summarises the growth and distribution of the vehicle population per Registering Authority.

All Authorities	2009/10	2010/11	2011/12	2012/13	2013/14	Distribution	% Growth
ARANOS	1,119	1,153	1,195	1,233	1,194	0.39%	-3.16%
BETHANIE	480	472	505	444	447	0.15%	0.68%
EENHANA	2,456	2,774	3,229	3,592	4,482	1.46%	24.78%
GOBABIS	5,628	5,906	6,263	6,343	6,574	2.14%	3.64%
GOVERNMENT	6,897	7,446	7,756	7,542	8,351	2.72%	10.73%
GROOTFONTEIN	3,903	4,100	4,283	4,254	4,608	1.50%	8.32%
KARASBURG	1,882	1,909	2,022	1,953	2,035	0.66%	4.20%
KARIBIB	1,006	1,087	1,145	1,166	1,271	0.41%	9.01%
KATIMA MULILO	2,450	2,809	3,321	3,599	3,942	1.29%	9.53%
KEETMANSHOOP	5,205	5,422	5,719	5,907	6,306	2.06%	6.75%
KHORIXAS	591	619	697	691	776	0.25%	12.30%
LÜDERITZ	1,998	2,023	2,110	2,143	2,201	0.72%	2.71%
MALTAHÖHE	554	555	577	568	541	0.18%	-4.75%
MARIENTAL	4,023	4,255	4,464	4,615	4,862	1.59%	5.35%
OKAHANDJA	4,969	5,146	5,394	5,532	5,976	1.95%	8.03%
OKAKARARA	475	510	616	631	627	0.20%	-0.63%
OMARURU	1,932	2,026	2,193	2,263	2,435	0.79%	7.60%
ONDANGWA	5,835	6,487	7,335	7,625	8,173	2.66%	7.19%
OPUWO	881	1,033	1,199	1,314	1,583	0.52%	20.47%
ORANJEMUND	2,244	2,204	2,198	2,394	2,593	0.85%	8.31%
ONGWEDIVA	15,909	17,538	19,811	20,574	22,373	7.29%	8.74%
OTAVI	1,129	1,114	1,176	1,108	1,043	0.34%	-5.87%
OTJINENE	213	221	255	232	277	0.09%	19.40%
OTJIWARONGO	6,358	6,688	7,075	7,421	8,170	2.66%	10.09%
OUTAPI	3,744	4,210	4,897	5,504	6,333	2.06%	15.06%
OUTJO	2,893	3,028	3,249	3,299	3,356	1.09%	1.73%
POL / NPS	146	201	217	199	200	0.07%	0.50%
REHOBOTH	3,278	3,371	3,632	3,713	4,015	1.31%	8.13%
RUNDU	5,210	5,859	6,788	7,258	8,020	2.61%	10.50%
SWAKOPMUND	13,179	14,358	15,367	16,122	16,988	5.54%	5.37%
TSUMEB	4,724	4,986	5,357	5,583	6,048	1.97%	8.33%
USAKOS	613	615	661	563	578	0.19%	2.66%
WALVIS BAY	14,247	15,254	16,462	16,908	17,935	5.85%	6.07%
WINDHOEK	110,965	118,967	128,336	133,999	142,388	46.44%	6.26%
TOTAL	**237,136**	**254,346**	**275,504**	**286,292**	**306,701**	**100.00%**	**7.13%**

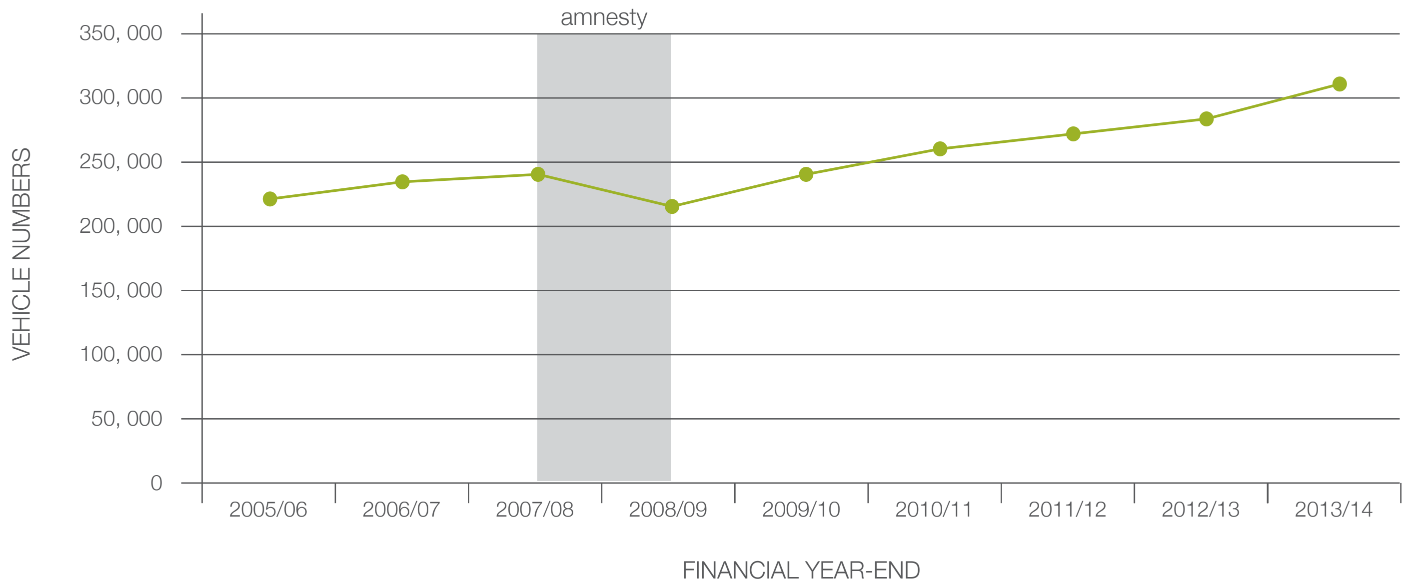
VEHICLE POPULATION DISTRIBUTION
Financial year-end



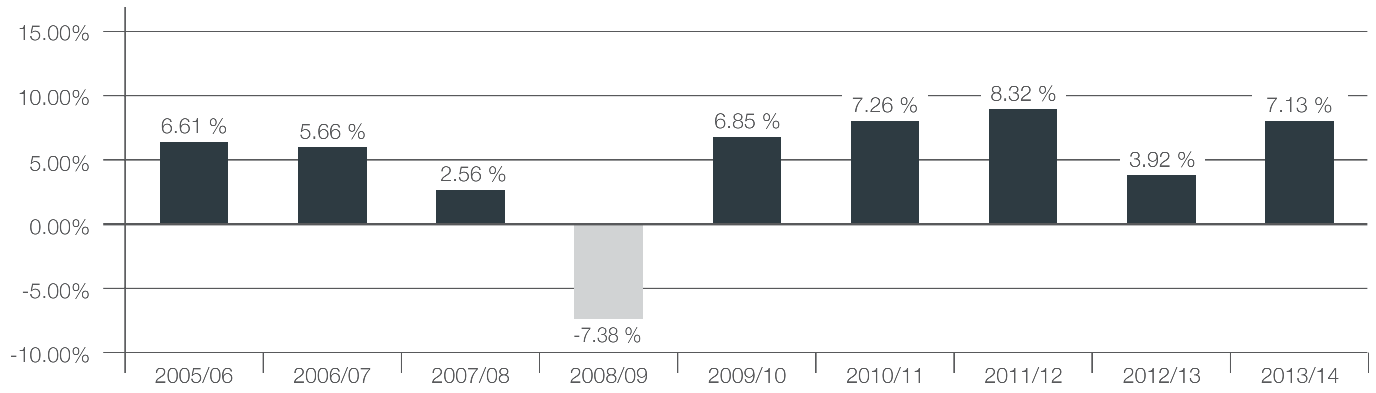
VEHICLE DISTRIBUTION BY TYPE
Financial Year-end



VEHICLE POPULATION



VEHICLE ANNUAL GROWTH NAMIBIA (%)

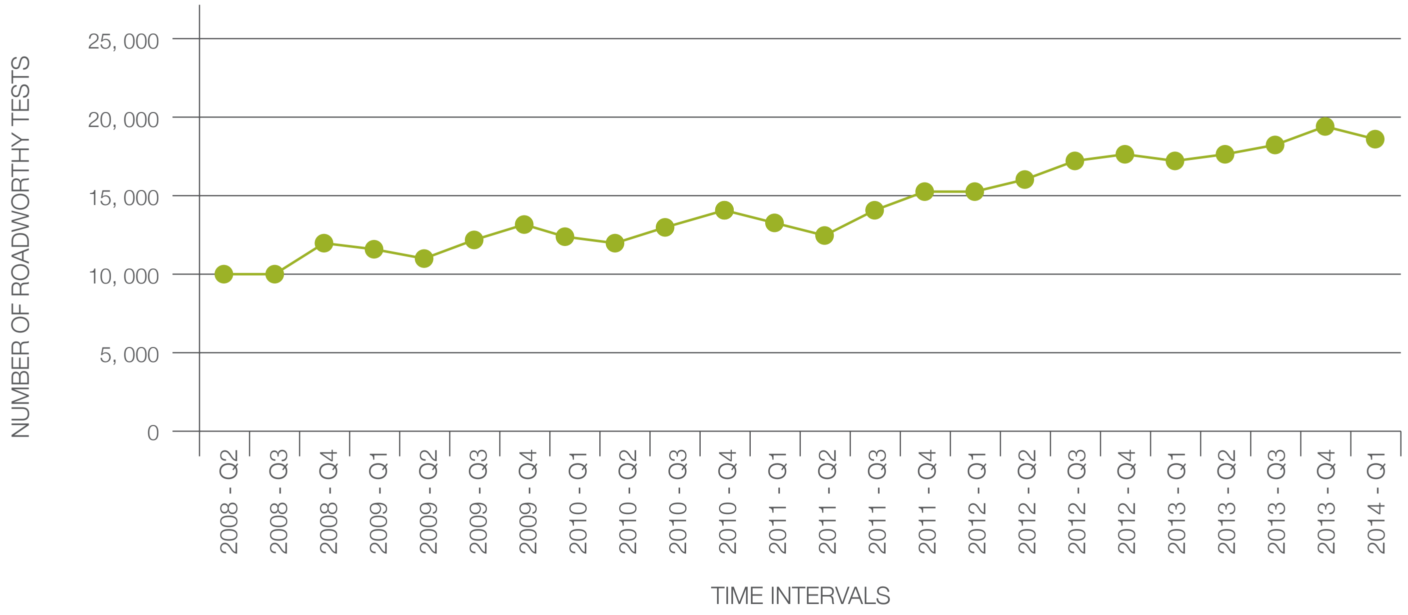


VEHICLE ROADWORTHY TESTING

The number of vehicle roadworthy tests increased by 19.76% to 73,358. The following table summarises the growth and distribution of the vehicle roadworthy tests per Vehicle Testing Station.

Vehicle Testing Station	All Vehicles (Tests) 2013/14					All Vehicles (Trends)				
	Bus	Goods Vehicle	Motor Cycle	Other	**Total**	2010/11	2011/12	2012/13	2013/14	**% Change**
EENHANA	9	39	2	655	**705**	0	0	0	705	**NA**
GOBABIS	17	664	3	1,148	**1,832**	1,272	1,449	1,548	1,832	**18.35%**
GROOTFONTEIN	21	306	6	900	**1,233**	925	62	0	1,233	**NA**
KARASBURG	13	42	0	267	**322**	279	311	293	322	**9.90%**
KARIBIB	37	183	1	180	**401**	326	428	484	401	**-17.15%**
KATIMA MULILO	9	63	0	645	**717**	594	638	740	717	**-3.11%**
KEETMANSHOOP	10	1	1	1,101	**1,113**	923	1,013	1,170	1,113	**-4.87%**
LÜDERITZ	43	60	1	359	**463**	387	388	428	463	**8.18%**
MARIENTAL	40	645	4	660	**1,349**	980	1,157	1,317	1,349	**2.43%**
OKAHANDJA	15	96	8	2,638	**2,757**	1,818	2,654	2,781	2,757	**-0.86%**
OPUWO	1	9	0	76	**86**	270	399	584	86	**-85.27%**
ORANJEMUND	72	266	3	280	**621**	409	428	563	621	**10.30%**
ONGWEDIVA	62	871	12	7,149	**8,094**	4,480	6,038	7,522	8,094	**7.60%**
OTJIWARONGO	71	1,003	19	1,388	**2,481**	1,531	1,782	2,192	2,481	**13.18%**
OUTJO	27	503	5	495	**1,030**	697	829	1,036	1,030	**-0.58%**
RUNDU	47	49	6	2,436	**2,538**	1,520	1,964	2,427	2,538	**4.57%**
SWAKOPMUND	139	1,236	61	2,352	**3,788**	3,475	3,919	3,904	3,788	**-2.97%**
TSUMEB	32	395	10	1,398	**1,835**	1,650	2,445	2,581	1,835	**-28.90%**
WALVIS BAY	11	18	16	5,451	**5,496**	3,389	3,681	4,901	5,496	**12.14%**
WINDHOEK	136	4,714	5	31,642	**36,497**	26,984	27,730	34,167	36,497	**6.82%**
TOTAL	**812**	**11,163**	**163**	**61,220**	**73,358**	**51,909**	**57,315**	**68,638**	**73,358**	**19.76%**

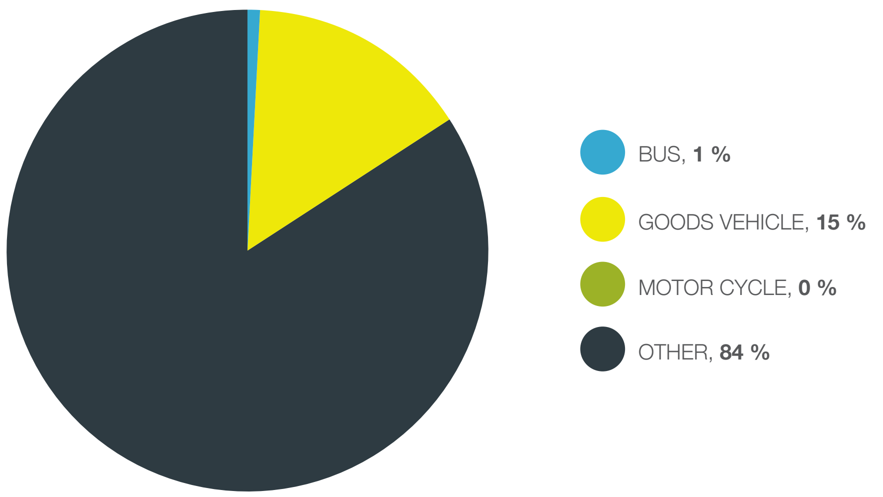
ROADWORTHY TEST
Quarterly



VEHICLES TESTED PER CATEGORY 2013/14

Test Period	Bus	Goods Vehicle	Motor Cycle	Other	**Total**
2013 - Q2	185	3,291	50	14,178	17,704
2013 - Q3	252	3,798	53	14,542	18,645
2013 - Q4	213	2,615	38	16,610	19,476
2014 - Q1	162	1,459	22	15,890	17,533
TOTAL	**812**	**11,163**	**163**	**61,220**	**73,358**
PERCENTAGE OF TOTAL	**1.11%**	**15.22%**	**0.22%**	**83.45%**	**100%**

DEMARCATION OF ROADWORTHY TESTS PER CATEGORY
2013/14

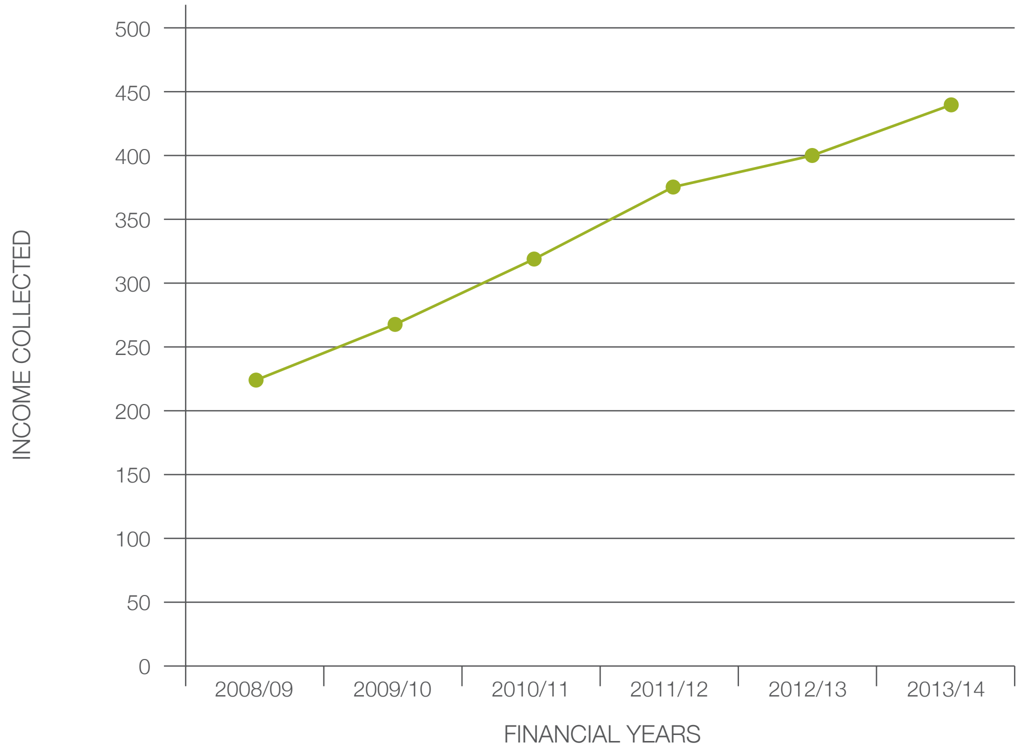


NaTIS TRANSACTION AND REVENUE COLLECTION

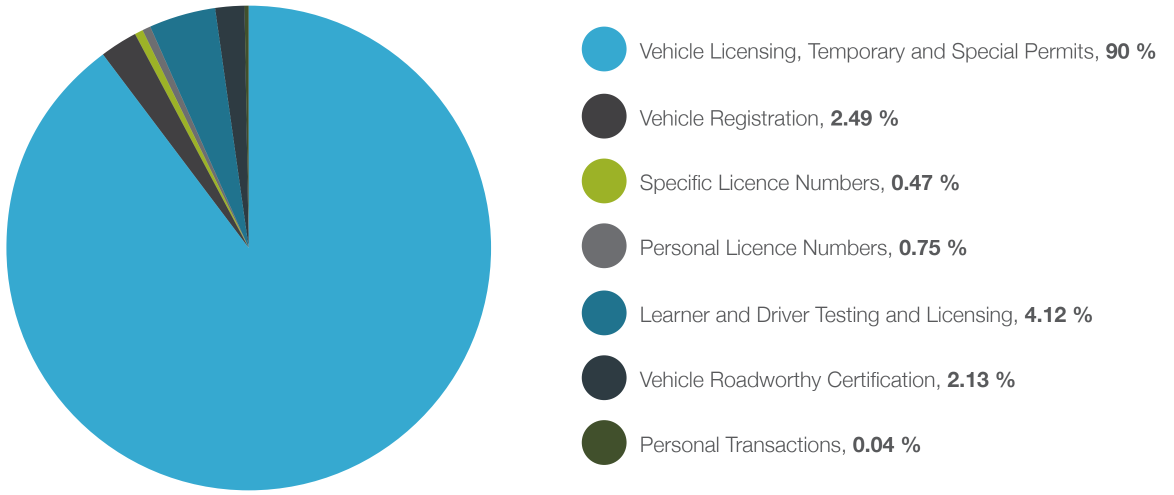
eNaTIS recorded 980,531 transactions for which money was collected. An income of N$438,329,558.94 was generated, with an increase of 9.13% compared to 2012/13. This amount is broken down as indicated in the table below.

Road User Charges	2012/13	2013/14	% Increase	Increase Amount	# Transactions
Vehicle Licensing, Temporary and Special Permits	N$ 361,842,361.42	N$ 394,505,226.94	9.03%	N$ 32,662,865.52	516,778
Non Road User Charges					
Vehicle Registration	N$ 9,773,221.00	N$ 10,927,166.00	11.81%	N$ 1,153,945.00	76,561
Specific Licence Numbers	N$ 1,754,256.00	N$ 2,080,220.00	18.58%	N$ 325,964.00	7,444
Personalised Licence Numbers	N$ 2,302,056.00	N$ 3,294,648.00	43.12%	N$ 992,592.00	2,221
Learner and Driver Testing and Licensing	N$ 17,168,775.00	N$ 18,080,290.00	5.31%	N$ 911,515.00	235,203
Vehicle Roadworthy Certification	N$ 8,706,456.00	N$ 9,327,000.00	7.13%	N$ 620,544.00	140,883
Personal Transactions	N$ 107,040.00	N$ 115,008.00	7.44%	N$ 7,968.00	1,441
Other (excluding Manual Transactions)	N$ 96.00	N$ 0.00	NA	N$ -96.00	0
TOTAL	**N$ 401,654,261.42**	**N$ 438,329,558.94**	**9.13%**	**N$ 36,675,297.52**	**980,531**

NaTIS REVENUE COLLECTION
5 year trend



NaTIS REVENUE COLLECTION DISTRIBUTION
2013/14



TRANSPORT REGULATION

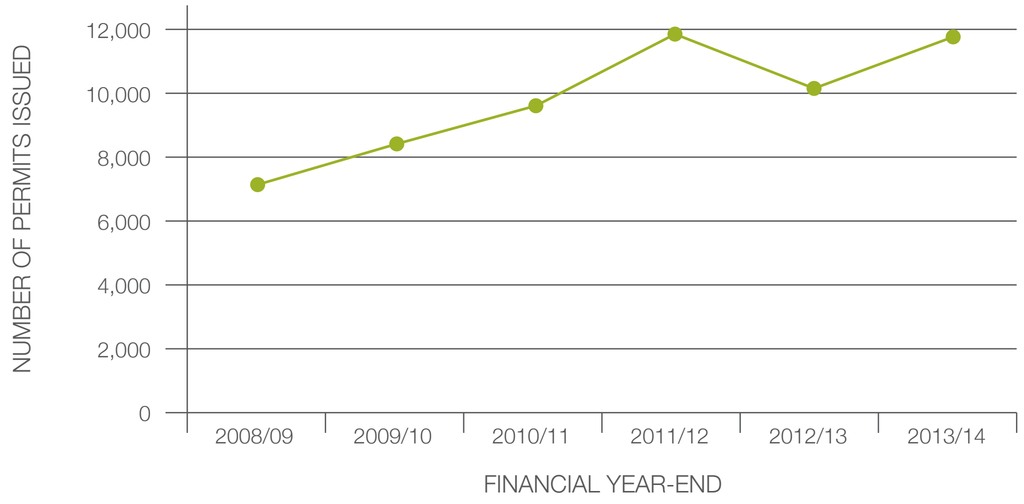
The Roads Authority regulated the cross border and domestic road transportation by issuing road carrier permits in compliance with the national legislative framework, bi-lateral and multi-lateral cross-border road transport agreements. Furthermore the organisation, processesd the application and issuing of abnormal permits.

STATISTICS OF CROSS-BODER ROAD CARRIER PERMITS

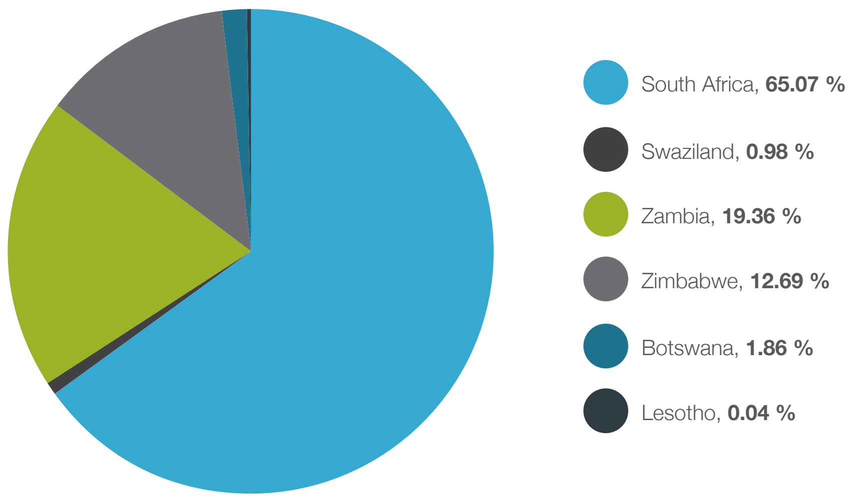
A total of 11,659 permits were issued, representing an increase of 13.05% compared to the previous year of 2012/13. The statistics per country is indicated in the table below. The increase in the number of cross road transportation permits is reflective of the economic situation within the region that is normalising.

Type of Permit	2009/10	2010/11	2011/12	2012/13	2013/14	Growth	% Growth
Botswana	164	128	268	211	217	6	2.84%
Lesotho	13	63	16	11	5	-6	-54.55%
South Africa	6,274	6,884	7,473	6,736	7,586	850	12.62%
Swaziland	115	124	135	50	114	64	128.00%
Zambia	1,065	1,447	2,449	2,078	2,257	179	8.61%
Zimbabwe	796	848	1,539	1,227	1,480	253	20.62%
Total	**8,427**	**9,494**	**11,880**	**10,313**	**11,659**	**1346**	**13.05%**

CROSS BORDER PERMITS ISSUED TO NEIGHBOURING COUNTRIES



CROSS BORDER PERMITS ISSUED PER COUNTRY
2013/2014

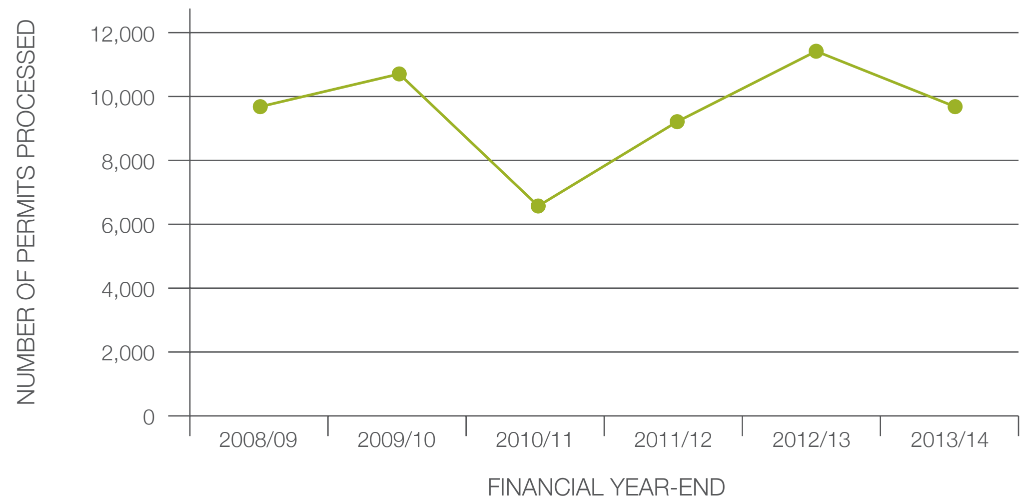


STATISTICS OF DOMESTIC ROAD CARRIER PERMITS

A total of 9,873 domestic road carrier permits, including temporary permits for Angolan foreign vehicles, were issued, representing a decrease of 12.94% compared to the previous year of 2012/2013. The statistics per type of applications / permits is indicated in the table below.

Type of Permit	2009/10	2010/11	2011/12	2012/13	2013/14	Growth	% Growth
New Application	2,282	2,045	2,129	3,397	3,433	36	1.06%
Replacement of Vehicle Application	27	5	4	7	4	-3	-42.86%
Temporary Permit Application locals	4,712	1,037	1,761	854	717	-137	-16.04%
Transfer of Permit Application	1,143	69	79	91	105	14	15.38%
Duplicate Permit Application	265	458	660	360	818	458	127.22%
Change of Route Application	27	33	29	30	39	9	30.00%
Additional Vehicle Application	4	10	3	6	1	-5	-83.33%
Additional Authority Application	44	19	5	4	15	11	275.00%
Direct Replacement Application	2,130	3,126	4,058	6,314	4,379	-1,935	-30.65%
Temporary Permit foreign vehicles	172	220	422	278	362	84	30.22%
Total	**10,806**	**7,022**	**9,150**	**11,341**	**9,873**	**-1,468**	**-12.94%**

DOMESTIC PERMITS PROCESSED



ISSUANCE OF ABNORMAL PERMITS

A total of 3,399 abnormal permit applications were processed, representing an increase of 68.94% compared to the previous year of 2012/2013.

Category	2008/09	2009/10	2010/11	2011/12	2012/13	2013/14	Growth	% Growth
Applications received	1,778	2,027	1,961	2,193	2,012	3,399	1,387	68.94%
Applications not issued/ cancelled	213	207	198	499	263	1,127	864	328.52%
Applications issued	1,565	1,820	1,763	1,694	1,749	2,272	523	29.90%
Paid	1,559	1,815	1,757	1,690	1,743	2,269	526	30.18%
GRN not paid	6	5	6	4	6	3	-3	-50.00%

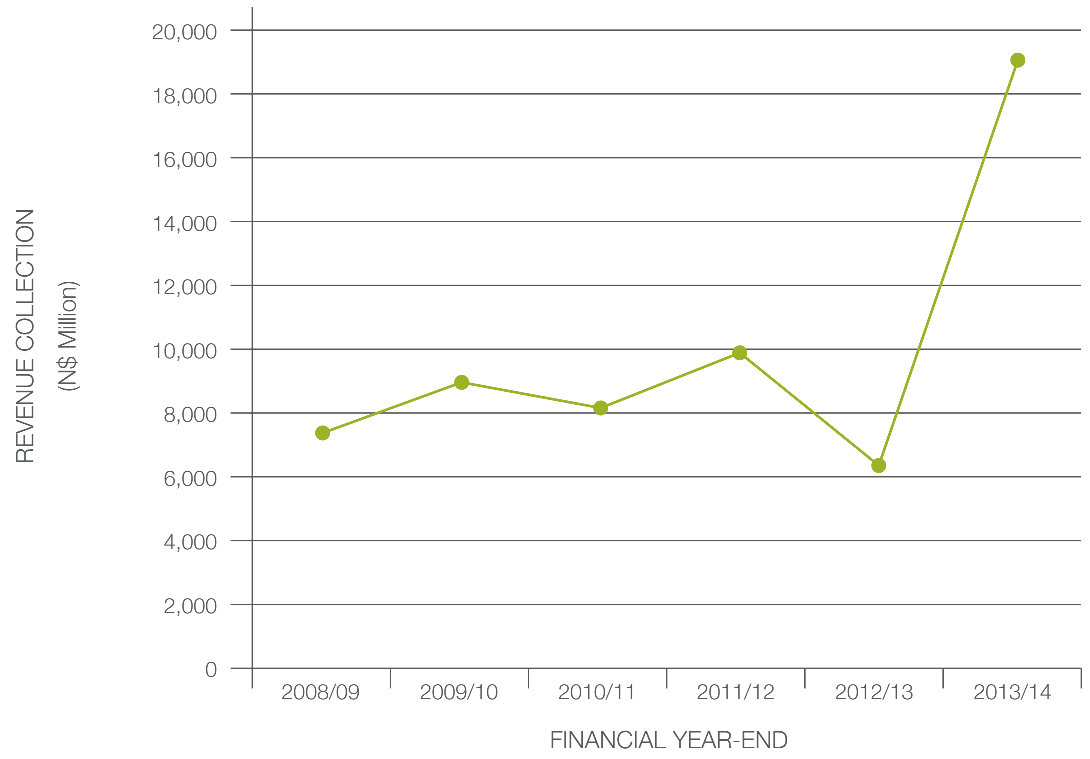
ABNORMAL LOAD PERMITS PROCESSED

REVENUE COLLECTION FOR TRANSPORT REGULATION

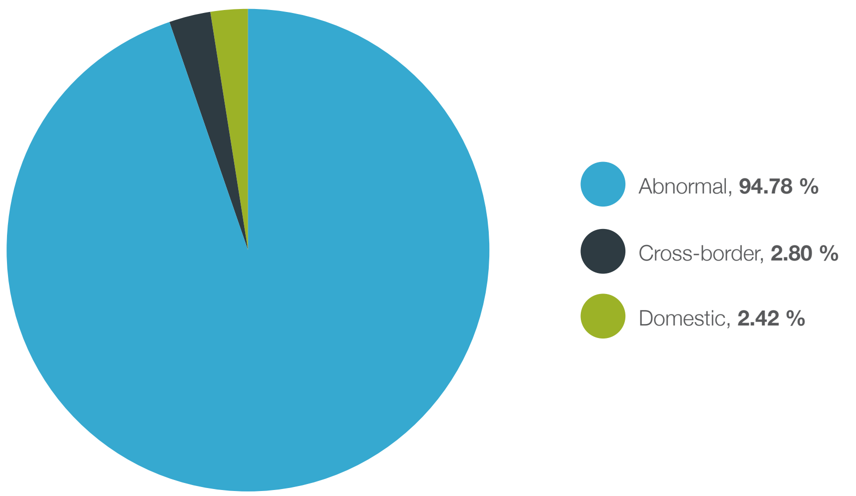
The 187% increase in the revenue is directly related to the increase in the number of abnormal permits transactions that were processed.

Category	2009/10	2010/11	2011/12	2012/13	2013/14	N$ Change	% Change
Cross border	N$ 384,250	N$ 432,950	N$ 474,750	N$ 480,550	N$ 538,160	N$ 57,610	12%
Domestic	N$ 979,386	N$ 351,575	N$ 490,545	N$ 366,475	N$ 465,745	N$ 99,270	27%
Abnormal	N$ 7,689,902	N$ 7,370,100	N$ 8,671,511	N$ 5,862,428	N$ 18,232,287	N$ 12,369,859	211%
Total	**N$ 9,053,538**	**N$ 8,154,625**	**N$ 9,636,806**	**N$ 6,709,453**	**N$ 19,236,192**	**N$ 12,526,739**	**187%**

TRANSPORT REGULATION REVENUE COLLECTION



TRANSPORT REGULATION: REVENUE COLLECTION DISTRIBUTION
Total: N$ 19,236,192



ROAD TRAFFIC AND TRANSPORT INSPECTORATE

STAKEHOLDER SATISFACTION

The Roads Authority actively engaged stakeholders to enhance strategic partnering through the facilitation of regional and international bodies - such as ASANRA as the implementation body of SADC; the Integrated Border Management Committee under Customs and Excise; the Multilateral Route Management Group, consisting of all SADC countries; the Walvis Bay Corridor Group and the Trans Kalahari Corridor.

ROAD NETWORK MANAGEMENT

Reduction of Damage due to Overloading

During the reporting period, the Roads Authority weighed 368,048 heavy vehicles, exceeding its annual target of 354,356 vehicles by 3.86% (13,692 vehicles). Out of 368,048 vehicles weighed, 9.8% (36,015) were overloaded, of which 0.9% (3,441) exceeded the 5% tolerance, while 8.9% (32,574) were overloaded but within the 5% tolerance.

The level of overloading increased by 0.9% from 8.9% in 2013 to 9.8% in 2014 due to the tolerance, which is not prosecutable and difficult to control.

The compliance rate therefore decreased from 91.1% in 2013 to 90.2% in 2014.

Weighbridge	Target	Vehicles weighed	Vehicles overload within 5%	Vehicles overload above 5%	Total vehicles overload	Total vehicles charged	Percentage overload
Brakwater	85 960	91,614	5,703	655	6,358	0	6.9
Aris	73 844	75,613	3,214	413	3,627	0	4.8
Gobabis	27 328	45,066	863	169	1,032	0	2.3
Walvis Bay	60 000	50,228	10,402	725	11,127	0	22.2
Onhuno	35 332	29,426	1,809	270	2,079	0	7.1
Noordoewer	6 784	6,277	460	29	489	0	7.8
Ariamsvlei	12 500	10,070	884	259	1,143	0	11.4
Rosh Pinah	1 512	1,073	128	31	159	0	14.8
Oshivelo	35 276	40,799	6,441	593	7,034	0	17.2
Katima	15 820	17,882	2,670	297	2,967	0	16.6
TOTAL	354 356	368,048	32,574	3,441	36,015	0	9.8

Table 1. Overload Control Statistics for Financial Year 2013/14

Commercial Vehicle Compliance to Road Traffic & Transport Legislation

Road Traffic Regulations

The Roads Authority inspected 119,600 commercial vehicles for compliance to road traffic regulations on road safety and vehicle/load dimensions.

Out of 119,600 vehicles inspected, 2.1% (2,519) were not in compliance and were charged. The compliance rate is reflected at 97.9%.

ENFORCEMENT ON ROAD TRAFFIC SYSTEM AND VEHICLE DIMENSION

Regional offices	Vehicle inspected	Vehicle charged	% Charged
Brakwater	6,084	354	5.8
Aris	12,286	465	3.8
Gobabis	5,309	229	4.3
Walvis Bay	3,757	390	10.4
Onhuno	21,249	375	1.8
Noordoewer	7,048	44	0.6
Ariamsvlei	10,983	59	0.5
Rosh Pinah	1,074	87	8.1
Oshivelo	36,828	373	1.0
Katima Mulilo	14,982	143	1.0
Total	119,600	2,519	2.1

Table 2. Road Traffic Statistics for 2013/14

Road Transportation

A total 209,971 commercial vehicles were inspected for cross-border road transport permits; domestic road carrier permits; cross-border charge permits and mass distance charge permits. Out of 209,971 vehicles inspected, 0.8% (1,645) were not in compliance and were charged.

ENFORCEMENT ON ROAD TRANSPORTATION AND CROSS-BORDER ENTRY FEE CHARGES AND MASS DISTANCE CHARGES

Regional offices	Vehicles inspected	Vehicles charged	% Charged
Brakwater	3,920	511	13.0
Aris	11,506	502	4.4
Gobabis	6,184	286	4.6
Walvis Bay	14,329	92	0.6
Onhuno	11,000	91	0.8
Noordoewer	10,614	14	0.1
Ariamsvlei	18,698	22	0.1
Rosh Pinah	1,178	6	0.5
Oshivelo	77,033	64	0.1
Katima Mulilo	55,509	57	0.1
Total	209,971	1,645	0.8

Vehicle compliance thus decreased from 99.3% in 2013 to 99.2% in 2014.

FINANCIAL MANAGEMENT

Fines Collection

A total amount of N$3,326,415 in admission of guilt fines was generated during the period under review, out of which 46.3% (N$1,539,110) was paid.

Type	Total Amount Of Fines Issued	Total Fines Paid
Fines	N$3,326,415	N$1,539,110

Budget Performance

Type of Budget	Budget Amount	Amount Spent	% Spent	N$ (%) Over/Under(-) Spent
Administration	34,009,833	30,958,086.15	91%	-3,051,746.85 (9%)
Operational	5,456,635	4,321,809.93	79.2%	-1,134,825.07 (20.8%)
Total	39,466,468	35,279,896.08	89.4%	-4,186,571.92 (10.6%)

NETWORK PLANNING AND CONSULTATION

FIVE-YEAR BUDGET 2013/14-2018/19

The five-year budget for the period 2013/14 – 2017/15 covering the management of the road network and the administration costs of the Roads Authority was prepared and finalised during the period under review. This budget is prepared on the basis of the Medium to Long Term Roads Master Plan (MLTRMP), taking into consideration the funding limitations as communicated by the Road Fund Administration.

The 2013/14 financial year budget was an amount of N$2,789,813,225, comprising of N$1,543,724,225, of which 55.3% was funded by RFA; and N$1,246,089,000 funded by Government, which represents 44.7% of the total budget. Out of the total amount of N$1,543,724,225 funded by the Road Fund Administration (RFA), about 81.08%, which is N$1,251,674,225, is for operations, while 18.92% is for general administration.

REVISION OF THE MEDIUM TO LONG TERM ROADS MASTER PLAN

The Medium to Long Term Roads Master Plan (MLTRMP) of 2003 provided the guidelines for the preparation of the five-year programme and budget for the Authority. The Master Plan identified road development and maintenance programmes over a 20-year period. The MLTRMP was revised during the financial year 2011/12 – 2013/14 and it was completed during that period. The achievement rate of the MLTRMP is 100%.

Revision of the Oshikoto, Oshana, Ohangwena, Omusati and Kavango Regions Road Master Plan, as well as special Review of Roads Programmes for //Karas and Hardap Regions including Tsumkwe Constituency in Otjozondjupa Region

The budgeted amount for this activity was N$1,500,000 during the 2013/14 financial year. Not all funds were utilised before the end of the financial year due to the fact that the tender was awarded in January 2014 and this resulted in the expenditure of N$1,000,000 only. The remainder was not spent during the 2013/14 financial year and will be spent during 2014/15 financial year as the study continues. The target of 50% was set and so far it has achieved 55%, which is well above the target.

ROADS BOARDS

- During the period under review, 16 Roads Boards meetings were held throughout the country and ±6,000 km were covered in the process.

- The Roads Boards recommended the proclamation of 19 district roads; the closing of 2 district roads, 2 farm roads, 1 minor road; and the deviation of 1 district road.

IMPLEMENTATION OF ASSET MANAGEMENT SYSTEM

This is a new regional project and; thus far, the self-assessment was done as well as the external assessment. The results of these assessments were presented on 15 January 2014 in Pretoria and a go-ahead for the drafting of the asset management policy was given on 16 January 2014. The completion of the system is set to be in 2015/2016 Financial Year.

RESEARCH AND DEVELOPMENT STRATEGY IMPLEMENTATION

The following took place with regard to Research and Development Strategy Implementation:

- The Research and Development Section completed an article on the "Use of Seawater for Compaction of Bitumen-surfaced Pavements."
- A paper on Carbonation on Kongola to Katima Mulilo Road has been completed.
- The Road Stabilisers Environmental Acceptance Guidelines has been completed.

TECHNOLOGY TRANSFER CENTRE

The Technology Transfer Centre was launched during the reporting period. The Roads Authority Technology Transfer Centre formed part of the International Organising Committee (IOC) that hosted the successful 6th Africa Technology Transfer Conference in Gaborone, Botswana. It has also initiated a Memorandum of Agreement with the Texas Transportation Institute for the purpose of future collaboration with the institution.

The Roads Authority is currently in negotiations with the US Federal Highway, AASHTO, NLTAPA (National Local Technical Assistance Program Association) with regards to a Memorandum of Agreement on exchange of knowledge. Lastly, it has a twinning agreement with the North Dakota Northern Tribal Technical Assistance Program. The Centre has established strong relationships with Regional and International Technology Transfer Centers.

The Roads Authority is currently compiling a Road Transportation Sustainability Plan for Namibia and it is in its final stages.

FEASIBILITY STUDIES

- Review of the Basic Planning Study for TO901 & TO601 Windhoek – Hosea Kutako International Airport Future Trunk Road.
- Road Safety and Traffic Management Study for TO106 & TO901 Windhoek Bypass and TO 1011 & MO92 through Ondangwa, Ongwediva, and Oshakati.
- Feasibility Study: Project Proposals for Partial Improvements of TO201: Walvis Bay – Swakopmund and the Upgrading to Bitumen Standards of MO044; Walvis Bay – Swakopmund (the Road behind the Dunes).
- Feasibility Study for the Upgrade of the Aus/Bethanie – Solitaire – Walvis Bay Road Link.
- Feasibility Study for the Rehabilitation of Trunk Roads TO102 Grünau – Mariental & TO103 Keetmanshoop – Mariental.
- A three year rehabilitation program for structures in critical and warning condition has been compiled.

ABNORMAL VEHICLE/LOAD PERMITS

- The Roads Authority processes Abnormal Transport Permits applications on a daily basis. The total number of 2,272 from the 3,399 applications received were processed and issued during the period under review. The abnormal permit fees collected for the Road Fund Administration amounted to N$18,232,287.22.

PROCLAMATIONS, COMPENSATION AND FENCING

The Roads Authority is responsible for the assessment and payment of compensation to affected parties as a result of road development. The Roads Authority processed requests for fencing along proclaimed roads and effected payments thereof in respect of the legislation. The proclamation, closure, deviation and reclassification of roads are processed as per needs and requests from Roads Boards and submitted to the Minister for approval.

The total amount allocated to Fencing was N$9,340,000.00. 100% of the funds were utilised for this purpose.

ROAD SAFETY MEASURES

During the period under review, the following activities were done with regard to Road Safety Measures:

- A Road Safety Measure preliminary design in Rundu was completed.
- The Roads Authority commenced with the construction of an Arrestor Bed at Usakos and the upgrading of three Intersections on TO202 near Swakopmund.
- The Roads Authority completed the 2+1 Cross Section Design as a Contribution to a safer Namibian Road Network and other Road Safety Related Initiatives in house study. This study was finalised under the reporting period and it has been send to relevant stakeholders for input.

ROAD CONSTRUCTION AND REHABILITATION

The list of projects presented below relates to projects planned or undertaken by the Roads Authority during the 2013/14 Financial Year.

COMPLETED ROADS AND BRIDGES CONSTRUCTION PROJECTS IN 2013/14:

- **TR 15/1: Tsumeb – Katwitwi, Section C Road Upgrading (67.3 km)**
  Contract Value worth N$275,593,500.06 and constructed by RCC/CHICO JV (China Henan International Group Limited). The project started September 2011 and was completed in October 2013.

- **Ovitoto Bridges Construction**
  Contract Value worth N$63,131,256 and constructed by RCC. Construction works started September 2012 and were completed in February 2014.

- **Okandjengedi Bridge Rehabilitation (18.55 m)**
  Contract Value worth N$44,771,585 and constructed by CHICO (Pty) Ltd.
  Construction works started August 2011 and were completed in December 2013.

- **Ongwediva Bridge Rehabilitation (61 m)**
  Contract Value is part of Okandjengedi Bridge and was constructed by CHICO (Pty) Ltd.
  Construction works started August 2011 and were completed in December 2013.

- **DR 3448: Kaisosi – Cuma Labour-based Gravel Road Construction (59 km)**
  Contract Value worth N$35,030,650 and constructed by Brandberg Construction (and 3 SME contractors).
  Construction works started November 2011 and were completed in June 2013.

- **DR 3671: Okatana – Ongwediva – Onamutai Labour-based Gravel Road Construction (17 km)**
  Contract Value worth N$42,616,017 and constructed by Thohi Construction (and 7 SME contractors). Construction works started September 2011 and were completed in August 2013.

- **DR 3672: Outapi – Okapalelona Border Post Labour-based Gravel Road Construction (14km)**
  Contract Value worth N$18,125,824 and constructed by EBC Contractors (and 3 SME contractors).
  Construction works started September 2011 and were completed in August 2013.

- **DR 3653: Oshikuku – Ekangolinene Labour-based Gravel Road Construction (26 km)**
  Contract Value worth N$37,993,341 and constructed by Nexus Civils (and 6 SME contractors).
  Construction works started December 2011 and were completed in April 2013.

Ongoing Roads and Bridges Construction Projects:

- **TR 1/6: Windhoek – Okahandja Road Rehabilitation, Section 3 (from end of dual carriageway to Dobra River Bridge, (8 km)**
  Contract Value worth N$239,710,110.27 and constructed by Grineker LTA. The project has reached 10% completion.
  Construction works started January 2014 and is expected to be completed in August 2015.

- **TR 14/2: Otjinene – Grootfontein Road Upgrading (238km)**
  Contract Value worth N$575,008,824.86 and constructed by CHICO. The project has reached 10% completion.
  Construction works started January 2014 and is expected to be completed in April 2016.

- **MR 118: Rosh Pinah – Oranjemund Road Upgrading (98km)**
  Contract Value worth N$558,617,218.96 and constructed by Raubex. The project has reached 12% completion.
  Construction works started January 2014 and is expected to be completed in July 2016.

- **MR 91: Gobabis – Aminus – Aranos Road Upgrading (250km)**
  Contract Value worth N$980,000,000.00 and constructed by RCC. Construction works started February 2014 and is expected to be completed in November 2016.

- **DR 3603: Onayena – Okankolo Road Upgrading (31km)**
  Contract Value worth N$ 103,877,096.51 and constructed by Nexus Civils. The project has reached 13% completion.
  Construction works started January 2014 and is expected to be completed in January 2015.

- **DR 3615: Onamutuku (Olwani) – Oshikuku Road Upgrading (16km)**
  Contract Value worth N$65,571,906.35 and constructed by Onamagongwa Trading Enterprises. The project has reached 35% completion.
  Construction works started August 2013 and is expected to be completed in February 2015.

- **DR 3668: Okalongo Extention – Okasamane Gate (Border Post) Road Upgrade (7km)**
  Contract Value worth N$34,048,918.02 and constructed by Namibia Investment Contracts CC, Tix Investment CC and Dolly Investments Joint Venture. The project has reached 5% completion.
  Construction works started November 2013 and is expected to be completed in April 2015.

- **MR 67: Omakange – Ruacana Road Upgrading (85km)**
  Contract Value worth N$419,135,795 and constructed by China Machinery Engineering Corp. The project has reached 25% completion.
  Construction works started August 2013 and is expected to be completed in February 2016.

- **DR 3608: Omafo – Ongenga – Outapi Road Upgrading (98km)**
  Contract Value worth N$722,200,000 and constructed by China Gezhouba International Engineering. The project has reached 30% completion. Construction works started August 2013 and is expected to be completed in August 2016.

- **MR 125: Liselo – Linyanti – Kongola – Singalamwe Road Upgrading (210km)**
  Contract Value worth N$828,460,556.00 and constructed by RCC & MCC Joint Venture. The project has reached 65% completion.
  Construction works started January 2012 and is expected to be completed in March 2015.

- **MR 120: Okatana – Endola – Onunho Road Upgrading (35km)**
  Contract Value worth N$186,999,256 and constructed by Namibia Road Products. The project has reached 70% completion.
  Construction works started July 2012 and is expected to be completed in March 2015.

- **MR 121: Eenhana – Oshigambo Road Upgrading (49km)**
  Contract Value worth N$ 193,206,689 and constructed by Namibbton/KL Construction Joint Venture. The project has reached 15% completion. Construction works started January 2014 and is expected to be completed in April 2015.

- **Tsumis River Bridge (0157) on TR 1/4: Rehoboth – Mariental (29 m)**
  Contract Value worth N$ 43,997,950 and constructed by Technoram Bridge & Roads. The project has reached 35% completion.
  Construction works started October 2013 and is expected to be completed in February 2015.

- **Tributary (Rooidam) River Bridge (2312) on TR 1/4: Rehoboth – Mariental (24m)**
  Contract Value is part of Tsumis Bridge and was constructed by Technoram Bridge & Roads. The project has reached 35% completion.
  Construction works started October 2013 and is expected to be completed in February 2015.

- **Mansguber River Bridge (158) on TR 1/4: Rehoboth – Mariental (24m)**
  Contract Value is part of Tsumis Bridge and was constructed by Technoram Bridge & Roads. The project has reached 35% completion.
  Construction works started October 2013 and is expected to be completed in February 2015.

- **DR 3524: Ngoma (Izimwe) – Nakabolelwa Labour-Based Gravel Road Construction (24km)**
  Contract Value worth N$46,158,809 and constructed by Kubaraf Development Enterprises (and 4 SME contractors). The project has reached 55% completion. Construction works started January 2012 and is expected to be completed in March 2015.

- **DR 3673: Omuthiya – Onanke Labour-Based Gravel Road Construction (34km)**
  Contract Value worth N$39,429,551 and constructed by Profile Civil Construction (and 4 SME contractors). The project has reached 65% completion.
  Construction works started February 2012 and is expected to be completed in December 2014.

- **DR 3449: Tjova – Divayi Labour-Based Gravel Road Construction (28km)**
  Contract Value worth N$36,731,744 and constructed by Ongombe Safaris and Logistics (and 2 SME contractors). The project has reached 95% completion.
  Construction works started November 2011 and is expected to be completed in November 2014.

- **DR 3427: Kamupupu – Mbururu Labour-Based Gravel Road Construction (21km)**
  Contract Value worth N$26,818,473 and constructed by Ongombe Safaris and Logistics (and 2 SME contractors). The project has reached 85% completion.
  Construction works started July 2012 and is expected to be completed in December 2014.

- **DR 3670: Oshandi – Eembahu – Oshiweda Labour-Based Gravel Road Construction (40km)**
  Contract Value worth N$53,551,173.40 and constructed by KL Construction (and 5 SME contractors). The project has reached 60% completion.
  Construction works started January 2013 and is expected to be completed in November 2014.

- **DR 3657: Oshapapa (Oshali) – Epumbu Labour-Based Gravel Road Construction (32km)**
  Contract Value worth N$46,158,809 and constructed by Nexus Civils (and 5 SME contractors). The project has reached 90% completion.
  Construction works started January 2013 and is expected to be completed in November 2014.

- **DR 3674: Onayena – Omahenge Labour-Based Gravel Road Construction (25km)**
  Contract Value worth N$45,568,471 and constructed by Namibbeton (and 5 SME contractors). The project has reached 70% completion.
  Construction works started March 2013 and is expected to be completed in November 2014.

- **DR 3649: Onalulago – Epembe Labour-Based Gravel Road Construction (42km)**
  Contract Value worth N$53,583,719 and constructed by Indigenous Construction (and 6 SME contractors). The project has reached 30% completion.
  Construction works started August 2013 and is expected to be completed in March 2015.

Planned Roads and Bridges Construction Projects for FY 2014/15:
The Designs and construction of following roads and bridges are envisaged to commence in the 2014/15 financial year:

- **DR 3609: Oshakati – Omungwelume – Ongenga Road upgrade (37 km)**
  This road is under design and construction is expected to commence in February 2014.

- **MR 44: Swakopmund – Walvis Bay Road Upgrading (43km)**
  The design for this road will start by July 2014 and construction by March 2015.

- **TR 1/6: Grunau – Keetmanshoop – Mariental Road Rehabilitation (386km)**
  The design for this road will start by September 2014 and construction by September 2015.

- **TR 10/2: Onhuno – Elundu Road Rehabilitation (72 km)**
  The design for this road will start by April 2014 and construction by April 2015.

- **MR 92: Ongwendiva – Ondangwa Road Rehabilitation (30 km)**
  The design for this road will start by April 2014 and construction by April 2015.

- **TR 1/11: Ondangwa – Omuthiya Road Rehabilitation (62 km)**
  The design for this road will start by April 2014 and construction by April 2015.

- **TR 1/12: Oshikango Bypass Road (17 km)**
  The design for this road will start by August 2014 and construction by June 2015.

- **MR 67: Swakopmund – Henties Bay Uis – Kamanjab Road Upgrading (412km)**
  The design for this road will start by June 2014 and construction by October 2015.

- **TR 2/1: Swakopmund – Walvis Bay (Coastal view) Road Rehabilitation (30 km)**
  The design for this road will start by October 2014 and construction by October 2015.

- **TR 1/5: Windhoek – Rehoboth Road Rehabilitation (84 km)**
  The design for this road will start by July 2014 and construction by July 2015.

- **DR 3508: Isize – Sifuha – Malaindi Schuckmannsburg Gravel Road (32 km)**
  The design for this road started in October 2013 and construction by November 2014.

- **DR 3683: Uukwiyuushona – Omutele Labour-Based Road Construction (48km)**
  The design for this road is complete and construction will start in January 2015.

- **DR 3681: Epato – Onaushe Labour-Based Road Gravel Construction (38km)**
  The design for this road is complete and construction will start in November 2014.

- **DR 3635: Amwaanda – Omutambomawe Labour-Based Gravel Road Construction (96km)**
  The design for this road is complete and construction will start in June 2014.

- **DR 3624: Etomba – Omundaungilo Labour-Based Road Gravel Construction (71km)**
  The design for this road is complete and construction will start in January 2015.

- **DR 4113: Endola – Eemboo Labour-Based Road Gravel Construction (19 km)**
  The design for this road will start in June 2014 and construction by March 2015.

- **DR 3650: Epinga – Onakalunga Labour-Based Road Gravel Construction (12 km)**
  The design for this road will start in June 2014 and construction by March 2015.

- **DR 3549: Masokotwani – Machita Gravel Road Construction (13km)**
  The design for this road will start by January 2014 and construction by August 2015.

- **DR 3424: Mungunda – Shakambu Gravel Road Construction (37km)**
  The design for this road will start by January 2015 and construction by July 2015.

- **DR 3445: Mpora – Bravo Gravel Road Construction (137km)**
  The design for this road will start by January 2015 and construction by August 2015.

CORPORATE SERVICES

HUMAN RESOURCES

During the period under review, the Roads Authority carried out activities revolving around the coordination of the implementation of the strategic plan, performance management, staffing of key positions and skills development.

Roads Authority Staff Complement and Staff Turnover

The organisational structural complement of the Roads Authority consists of:

- 622 positions
- 452 were staff in service as at 31 March 2014,
- 11 staff members were expatriates employed on a short-to-medium-term contract of 3-5 years in engineering positions.
- 68 employees were recruited from external sources
- 16 employees were promoted to higher positions
- 15 staff members resigned from the Roads Authority
- 6 employment contracts were terminated due to disciplinary action.

HUMAN RESOURCES MANAGEMENT AND ORGANISATIONAL TRANSFORMATION

As for organisational development and transformation, several projects were completed, while some are ongoing:

Strategic Plan 2012/2015

The second year of the current three-year strategic plan was reviewed.

Performance Management

The organisation continued to enhance the performance management system in order to strengthen support on the attainment of organisational goals and objectives as set out in the strategic plan.

Learning and skill's development

Efforts were made to:

- Promote and encourage staff development through job-specific training and development interventions and initiatives.
- Build capacity in the roads sector continued through the Roads Authority bursary scheme.
- Fund 37 students, who received sponsorship in various fields of studies, with the majority of students studying for civil engineering and transport management qualifications.
- Assist engineering graduates to gain the necessary practical exposure to enable them to obtain professional registration with the Engineering Council of Namibia through the Engineering Graduate Development Programme, whereby engineering graduates undergo mentorship and practical exposure.
- Implement leadership and management development programmes during the year under review.

SHARED SERVICES

Apart from the renovation of Regional and District Offices, a tender for the construction of the Roads Authority Head Office in Windhoek was awarded to the total amount of N$248,301,522.42 and the site was handed over to the contractor (Namibia Construction (Pty) Ltd) on 27 February 2014. It is expected that the construction will be completed by July 2016.

INFORMATION COMMUNICATION TECHNOLOGY (ICT)

Integrated Business Management System (IBMS)

During the year under review, the Roads Authority updated the IBMS to the latest technological platform (Microsoft Business Solomon – Dynamics SL 2011) to ensure continued product life support from Microsoft for IBMS maintenance.

Continuous improvements of the IBMS also took place, such as redeveloping and redeploying the improved reporting capability for financial and payroll compliance reporting needs.

The first phase of scoping and requirement specifications for the upgrade of the IBMS from Microsoft Dynamics SL 11 to Microsoft Dynamics AX 2012 was completed and the next phase of implementation is expected to be completed within this financial year.

AUDIT CERTIFICATE

AUDIT CERTIFICATE ON THE ACCOUNTS OF THE ROADS AUTHORITY FOR THE YEAR ENDED 31 MARCH 2014

The Board of Directors of the Roads Authority appointed the firm, Grand Namibia of Windhoek which is registered in terms of the Public Accountants and Auditors Act, 1951 (Act 51 of 1951). The said Firm compiled the audit documentation which was examined by me in terms of Section 21 (3) of the Roads Authority Act, 1999 (Act 17 of 1999).

The firm certified that:

"The scope of our audit was sufficient to support the opinion being issued.

The financial statements, except as noted in the audit report.

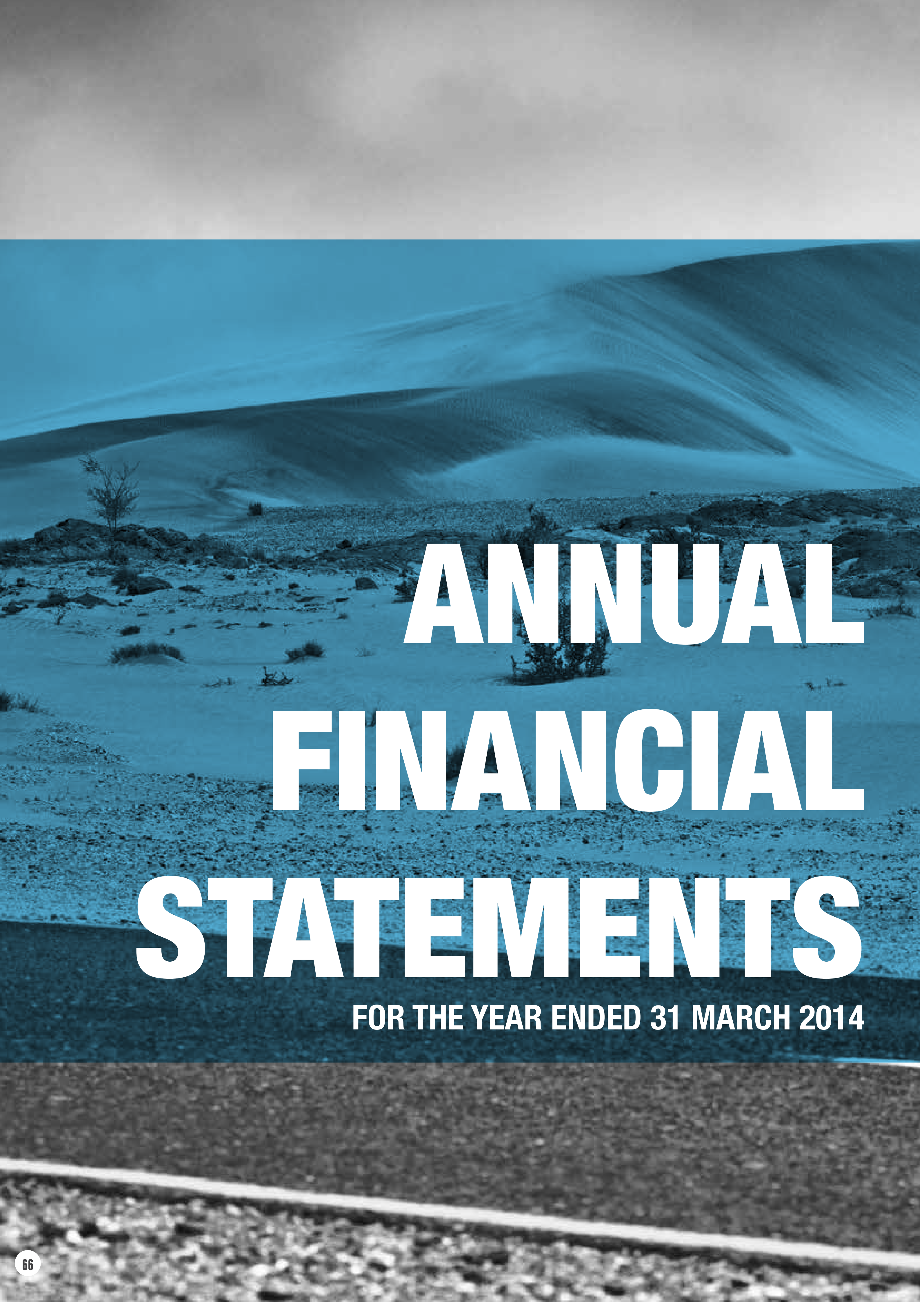
- *(i) are complete and clerically accurate;*
- *(ii) accord with our understanding of the client's business and industry;*
- *(iii) have been properly prepared in accordance with the Roads Authority Act and International Financial Reporting Standards; and*
- *(iv) fairly present the financial position, results of operations and cash flow information for the year then ended 31 March 2014."*

The audit of the financial year has been carried out to my satisfaction.

WINDHOEK, October 2014

JUNIAS ETUNA KANDJEKE
AUDITOR-GENERAL

AUDITOR-GENERAL
REPUBLIC OF NAMIBIA



ANNUAL FINANCIAL STATEMENTS

FOR THE YEAR ENDED 31 MARCH 2014

GENERAL INFORMATION

Country of incorporation and domicile
Namibia

Nature of business and principal activities
Management of the National Road Network of Namibia

Directors
H. Kaifanua (Chairperson)
M.E. Hanekom
L. Likando
E.S.T. Haipinge
B. Katjaerua

Registered office
Bell Street
Snyman Circle
Windhoek

Business address
Bell Street
Snyman Circle
Windhoek

Postal address
Private Bag 12030
Ausspannplatz
Windhoek
Namibia

Bankers
Bank Windhoek Limited

Auditors
Grand Namibia
Registered Accountants and Auditors
Chartered Accountants (Namibia)

INDEX

The reports and statements set out below comprise the financial statements presented to the shareholder:

Index Page

DIRECTORS' RESPONSIBILITIES AND APPROVAL

The directors are required in terms of the Roads Authority Act, Act 17 of 1999 to maintain adequate accounting records and are responsible for the content and integrity of the financial statements and related financial information included in this report. It is their responsibility to ensure that the financial statements fairly present the state of affairs of the Authority as at the end of the financial year and the results of its operations and cash flows for the period then ended, in conformity with International Financial Reporting Standards. The external auditors are engaged to express an independent opinion on the financial statements.

The financial statements are prepared in accordance with International Financial Reporting Standards and are based upon appropriate accounting policies consistently applied and supported by reasonable and prudent judgements and estimates.

The directors acknowledge that they are ultimately responsible for the system of internal control established by the Authority and place considerable importance on maintaining a strong control environment. To enable the directors to meet these responsibilities, the board of directors sets standards of internal control aimed at reducing the risk of error or loss in a cost-effective manner. The standards include the proper delegation of responsibilities within a clearly defined framework, effective accounting procedures and adequate segregation of duties to ensure an acceptable level of risk. These controls are monitored throughout the Authority and all employees are required to maintain the highest ethical standards in ensuring the Authority's business is conducted in a manner that in all reasonable circumstances is above reproach. The focus of risk management in the Authority is on identifying, assessing, managing and monitoring all known forms of risk across the Authority. While operating risk cannot be fully eliminated, the Authority endeavours to minimise it by ensuring that appropriate infrastructure, controls, systems and ethical behaviour are applied and managed within predetermined procedures and constraints.

The directors are of the opinion, based on the information and explanations given by management, that the system of internal control provides reasonable assurance that the financial records may be relied on for the preparation of the financial statements. However, any system of internal control can provide only reasonable, and not absolute, assurance against material mis-statement or loss.

The directors have reviewed the Authority's cash flow forecast for the year to 31 March 2015 and, in light of this review and the current financial position, they are satisfied that the Authority has or has access to adequate resources to continue in operational existence for the foreseeable future.

The external auditors are responsible for independently auditing and reporting on the Authority's financial statements. The financial statements have been examined by the Authority's external auditors and their report is presented on page 67.

The financial statements set out on pages 68 to 87, which have been prepared on the going concern basis, were approved by the board of directors on 12 September 2014 and were signed on its behalf by:

H. Kaifanua (Chairperson)

B. Katjaerua

INDEPENDENT AUDITORS' REPORT

To the Members of Roads Authority

We have audited the financial statements of Roads Authority, as set out on pages 68 to 87, which comprise the statement of financial position as at 31 March 2014, and the statement of comprehensive income, statement of changes in equity and statement of cash flows for the year then ended, and the notes, comprising a summary of significant accounting policies and other explanatory information.

Directors' Responsibility for the Financial Statements

The Authority's directors are responsible for the preparation and fair presentation of these financial statements in accordance with International Financial Reporting Standards, and requirements of the Roads Authority Act, Act 17 of 1999, and for such internal control as the directors determine is necessary to enable the preparation of financial statements that are free from material mis-statements, whether due to fraud or error.

Auditors' Responsibility

Our responsibility is to express an opinion on these financial statements based on our audit. We conducted our audit in accordance with International Standards on Auditing. Those standards require that we comply with ethical requirements and plan and perform the audit to obtain reasonable assurance whether the financial statements are free from material misstatement.

An audit involves performing procedures to obtain audit evidence about the amounts and disclosures in the financial statements. The procedures selected depend on the auditors' judgement, including the assessment of the risks of material misstatement of the financial statements, whether due to fraud or error. In making those risk assessments, the auditor considers internal control relevant to the entity's preparation and fair presentation of the financial statements in order to design audit procedures that are appropriate in the circumstances, but not for the purpose of expressing an opinion on the effectiveness of the entity's internal control. An audit also includes evaluating the appropriateness of accounting policies used and the reasonableness of accounting estimates made by management, as well as evaluating the overall presentation of the financial statements.

We believe that the audit evidence we have obtained is sufficient and appropriate to provide a basis for our audit opinion.

Opinion

In our opinion, the financial statements present fairly, in all material respects, the financial position of Roads Authority as at 31 March 2014, and its financial performance and its cash flows for the year then ended in accordance with International Financial Reporting Standards, and the requirements of the Roads Authority Act, Act 17 of 1999.

Grand Namibia

Grand Namibia
Registered Accountants and Auditors
Chartered Accountants (Namibia)

Per: R.N. Beukes

12 September 2014

9 Axali Doeseb Street
Windhoek
Namibia

DIRECTORS' REPORT

The directors have pleasure in submitting their report on the financial statements of Roads Authority for the year ended 31 March 2014.

1. Nature of business

The Authority is engaged in the management of the national road network of Namibia.

There have been no material changes to the nature of the Authority's business from the prior year.

2. Review of financial results and activities

The financial statements have been prepared in accordance with International Financial Reporting Standards and the requirements of the Roads Authority Act, Act 17 of 1999. The accounting policies have been applied consistently compared to the prior year.

Full details of the financial position, results of operations and cash flows of the Authority are set out in these financial statements.

3. Board and Sub-Committee meetings

	Board	Audit Board Committee	Board Tender Committee	HR Board Committee
Number of meetings	4	3	4	3
Directors				
Ms H. Kaifanua	4	-	4	3
Prof F. P. L. Kavishe	-	3	3	-
Ms M.E. Hanekom	4	3	4	3
Mr P.J. Maritz	4	-	4	-
Mr J.B. Mukoya	4	-	4	3
Mr G. Iitembu (*)	-	3	-	-

* Co-opted Member of the Audit Committee.

4. Directorate

The directors in office at the date of this report are as follows:

Directors	Term of service expired	Appointed
H. Kaifanua (Chairperson)	15 July 2014	15 July 2014 - Re-appointed
P.J. Maritz	15 July 2014	
Professor F.P.L. Kavishe	15 July 2014	
M.E. Hanekom	15 July 2014	15 July 2014 - Re-appointed
J.B. Mukoya	15 July 2014	
L. Likando		15 July 2014 - Appointed
E.S.T. Haipinge		15 July 2014 - Appointed
B. Katjaerua		15 July 2014 - Appointed

5. Events after the reporting period

The directors are not aware of any material event which occurred after the reporting date and up to the date of this report.

6. Going concern

The directors believe that the Authority has adequate financial resources to continue in operation for the foreseeable future and accordingly the financial statements have been prepared on a going concern basis. The directors have satisfied themselves that the Authority is in a sound financial position and that it has access to sufficient government funding to meet its foreseeable cash requirements. The directors are not aware of any new material changes that may adversely impact the Authority. The directors are also not aware of any material non-compliance with statutory or regulatory requirements or of any pending changes to legislation which may affect the Authority.

STATEMENT OF FINANCIAL POSITION AS AT 31 MARCH 2014

	Note(s)	2014 N$ '000	2013 Restated N$ '000	2012 Restated N$ '000
Assets				
Non-Current Assets				
Property, plant and equipment	4	58 735	47 805	45 326
Intangible assets	5	5 232	165	338
Prepayments	7	33 568	29 777	-
		97 535	**77 747**	**45 664**
Current Assets				
Trade and other receivables	8	255 777	219 680	249 804
Cash and cash equivalents	9	43 409	59 905	7 764
		299 186	**279 585**	**257 568**
Total Assets		**396 721**	**357 332**	**303 232**
Equity and Liabilities				
Equity				
Government contribution	10	8 992	8 992	8 992
Retained income		2 686	2 686	2 686
		11 678	**11 678**	**11 678**
Liabilities				
Non-Current Liabilities				
Finance lease obligation	11	4 959	6 678	5 430
Retirement benefit obligation	6	71 022	62 398	60 876
Deferred income	12	131 471	107 570	88 985
		207 452	**176 646**	**155 291**
Current Liabilities				
Finance lease obligation	11	3 762	3 359	3 127
Trade and other payables	14	154 284	150 989	120 828
Provisions	13	19 545	14 660	12 308
		177 591	**169 008**	**136 263**
Total Liabilities		**385 043**	**345 654**	**291 554**
Total Equity and Liabilities		**396 721**	**357 332**	**303 232**

STATEMENT OF COMPREHENSIVE INCOME

	Note(s)	2014 N$ '000	2013 Restated N$ '000	2012 Restated N$ '000
Revenue				
Grants received: Namibia Road Fund Administration		1 406 526	1 247 440	1 006 550
Interest received		1 826	1 363	1 814
	15	**1 408 352**	**1 248 803**	**1 008 364**
Other income				
Insurance claims		-	116	58
Sundry income		1 545	538	863
Tender Documents		1 178	683	1 089
		2 723	**1 337**	**2 010**
Operating expenses		**(1 164 527)**	**(1 029 853)**	**(820 997)**
Project administrative expenses		(6 048)	(8 559)	(6 191)
Routine and period maintenance		(860 617)	(790 450)	(644 188)
Construction and rehabilitation		(198 186)	(137 065)	(70 051)
Namibian Traffic Information Systems		(60 420)	(59 024)	(69 860)
Road management system		(18 145)	(15 359)	(14 717)
Research, development and feasibility studies		(7 960)	(9 711)	(7 126)
Fencing and compensation		(8 829)	(7 200)	(7 032)
Weigh bridge maintenance		(4 322)	(2 485)	(1 832)
Administrative expenditure		**(240 480)**	**(208 397)**	**(190 584)**
Employee cost	16	(160 584)	(147 481)	(130 476)
Other administrative expenses	16	(79 818)	(67 856)	(61 348)
Road Management qualification	17	(78)	(227)	(8 497)
Project cost		**(1 405 007)**	**(1 238 250)**	**(1 021 324)**
Operating surplus (deficit)		6 068	11 890	(10 950)
Finance costs	18	(909)	(1 022)	(952)
Surplus (deficit) for the year		**5 159**	**10 868**	**(11 902)**
Other comprehensive surplus:				
Items that will not be reclassified to profit or loss:				
Remeasurements on net defined benefit liability/asset		-	7 171	-
Transfer to Namibia Road Fund Administration				
Operating deficit (surplus)		(2 436)	(16 702)	13 912
Other income		(2 723)	(1 337)	(2 010)
Total transfers		**(5 159)**	**(18 039)**	**11 902**
Total comprehensive income for the year		**-**	**-**	**-**

STATEMENT OF CHANGES IN EQUITY

	Government contribution N$ '000	Retained income N$ '000	Total equity N$ '000
Balance at 1 April 2011	8 992	2 686	11 678
Deficit for the year	-	(11 902)	(11 902)
Transfer between reserves	-	11 902	11 902
Balance at 01 April 2012 as restated	**8 992**	**2 686**	**11 678**
Surplus for the year	-	10 868	10 868
Other comprehensive surplus	-	7 171	7 171
Total comprehensive income for the year	**-**	**18 039**	**18 039**
Transfer between reserves	-	(18 039)	(18 039)
Total Changes	**-**	**(18 039)**	**(18 039)**
Balance at 01 April 2013	**8 992**	**2 686**	**11 678**
Surplus for the year	-	5 159	5 159
Total comprehensive income for the year	**-**	**5 159**	**5 159**
Transfer between reserves	-	(5 159)	(5 159)
Balance at 31 March 2014	**8 992**	**2 686**	**11 678**

STATEMENT OF CASH FLOWS

	Note(s)	2014 N$ '000	2013 N$ '000	2012 N$ '000
Cash flows from operating activities				
Cash receipts from Road Fund Administration and customers		1 337 582	1 272 459	909 491
Cash paid to suppliers and employees		(1 324 678)	(1 181 862)	(903 735)
Cash (used in) generated from operations	19	12 904	90 597	5 756
Interest income		1 826	1 363	1 814
Finance costs		(909)	(1 022)	(952)
Net cash from operating activities		**13 821**	**90 938**	**6 618**
Cash flows from investing activities				
Purchase of property, plant and equipment	4	(17 402)	(5 301)	(4 594)
Leased Motor vehicles returned	4	-	234	-
Purchase intangible assets	5	(5 229)	-	-
Long - term prepayments	7	(3 789)	(29 778)	-
Net cash from investing activities		**(26 420)**	**(34 845)**	**(4 594)**
Cash flows from financing activities				
Finance lease payments-capital redemption		(3 896)	(3 661)	(3 507)
Finance lease obligation withdrawals		-	(291)	-
Net cash from financing activities		**(3 896)**	**(3 952)**	**(3 507)**
Total cash movement for the year		**(16 495)**	**52 141**	**(1 483)**
Cash at the beginning of the year		59 905	7 764	9 247
Total cash at end of the year	9	**43 410**	**59 905**	**7 764**

ACCOUNTING POLICIES

1. PRESENTATION OF FINANCIAL STATEMENTS

The financial statements have been prepared in accordance with International Financial Reporting Standards, and the Roads Authority Act, Act 17 of 1999. The financial statements have been prepared on the historical cost basis, and incorporate the principal accounting policies set out below. They are presented in Namibia Dollars.

These accounting policies are consistent with the previous period except for the change in accounting policy relating to IAS 19

1.1 Significant judgements and sources of estimation uncertainty

In preparing the financial statements, management is required to make estimates and assumptions that affect the amounts represented in the financial statements and related disclosures. Use of available information and the application of judgement is inherent in the formation of estimates. Actual results in the future could differ from these estimates which may be material to the financial statements. Significant judgements include:

Non-financial asset lives and residual values

The Authority assesses its trade receivables, held to maturity investments and loans and receivables for impairment at the end of each reporting period. In determining whether an impairment loss should be recorded in profit or loss, the Authority makes judgements as to whether there is observable data indicating a measurable decrease in the estimated future cash flows from a financial asset.

The impairment for trade receivables, held to maturity investments and loans and receivables is calculated on a portfolio basis, based on historical loss ratios, adjusted for national and industry-specific economic conditions and other indicators present at the reporting date that correlate with defaults on the portfolio. These annual loss ratios are applied to loan balances in the portfolio and scaled to the estimated loss emergence period.

Impairment of non-financial assets

The recoverable amounts of cash-generating units and individual assets have been determined based on the higher of value in use calculations and fair values less costs to sell. These calculations require the use of estimates and assumptions. It is reasonably possible that the assumption may change which may then impact our estimations and may then require a material adjustment to the carrying value of tangible assets.

The Authority reviews and tests the carrying value of assets when events or changes in circumstances suggest that the carrying amount may not be recoverable. In addition. Assets are grouped at the lowest level for which identifiable cash flows are largely independent of cash flows of other assets and liabilities. If there are indications that impairment may have occurred, estimates are prepared of expected future cash flows for each group of assets. Expected future cash flows used to determine the value in use of tangible assets are inherently uncertain and could materially change over time.

Provisions

Provisions were raised and management determined an estimate based on the information available. Additional disclosure of these estimates of provisions are included in note 13 - Provisions.

Leases

The Authority exercises judgement in classifying leases as operating- or finance leases based on the information available at the inception of the lease.

Post-employment benefit obligations

Actuarial valuations used to value post-employment benefit obligations, are based on assumptions which include employee turnover, mortality rates, the discount rate, healthcare inflation costs and the rates of increase in compensation costs.

1.2 Property, plant and equipment

The cost of an item of property, plant and equipment is recognised as an asset when:
- it is probable that future economic benefits associated with the item will flow to the Authority; and
- the cost of the item can be measured reliably.

Property, plant and equipment is initially measured at cost.

Costs include costs incurred initially to acquire or construct an item of property, plant and equipment and costs incurred subsequently to add to, replace part of, or service it. If a replacement cost is recognised in the carrying amount of an item of property, plant and equipment, the carrying amount of the replaced part is derecognised.

Property, plant and equipment are depreciated on the straight line basis over their expected useful lives to their estimated residual value.

Property, plant and equipment is carried at cost less accumulated depreciation and any impairment losses.

The useful lives of items of property, plant and equipment have been assessed as follows:

Item	Rates
Buildings	2%
Plant and machinery	6,66% - 33,33%
Motor vehicles, furniture and fittings	20%
Computer equipment	33,33%
Cellular phones	50%

The residual value, useful life and depreciation method of each asset are reviewed at the end of each reporting period. If the expectations differ from previous estimates, the change is accounted for as a change in accounting estimate.

The depreciation charge for each period is recognised in profit or loss unless it is included in the carrying amount of another asset.

The gain or loss arising from the derecognition of an item of property, plant and equipment is included in profit or loss when the item is derecognised. The gain or loss arising from the derecognition of an item of property, plant and equipment is determined as the difference between the net disposal proceeds, if any, and the carrying amount of the item.

1.3 Intangible assets

An intangible asset is recognised when:
- it is probable that the expected future economic benefits that are attributable to the asset will flow to the entity; and
- the cost of the asset can be measured reliably.

Intangible assets are initially recognised at cost.

Intangible assets are carried at cost less any accumulated amortisation and any impairment losses.

An intangible asset is regarded as having an indefinite useful life when, based on all relevant factors, there is no foreseeable limit to the period over which the asset is expected to generate net cash inflows. Amortisation is not provided for these intangible assets, but they are tested for impairment annually and whenever there is an indication that the asset may be impaired. For all other intangible assets amortisation is provided on a straight line basis over their useful life.

The amortisation period and the amortisation method for intangible assets are reviewed every period-end.

Reassessing the useful life of an intangible asset with a finite useful life after it was classified as indefinite is an indicator that the asset may be impaired. As a result the asset is tested for impairment and the remaining carrying amount is amortised over its useful life.

Amortisation is provided to write down the intangible assets, on a straight line basis, to their residual values as follows:

Item	Useful life
Computer software	3 years

1.4 Financial instruments

Classification

The Authority classifies financial assets and financial liabilities into the following categories:
- Financial assets at fair value through profit or loss - held for trading
- Loans and receivables
- Financial liabilities measured at amortised cost

Classification depends on the purpose for which the financial instruments were obtained / incurred and takes place at initial recognition. Classification is re-assessed on an annual basis, except for financial assets designated as at fair value through profit or loss, which shall not be classified out of the fair value through profit or loss category.

Initial recognition and measurement

Financial instruments are recognised initially when the Authority becomes a party to the contractual provisions of the instruments.

The Authority classifies financial instruments, or their component parts, on initial recognition as a financial asset, a financial liability or an equity instrument in accordance with the substance of the contractual arrangement.

Financial instruments are measured initially at fair value, except for equity investments for which a fair value is not determinable, which are measured at cost and are classified as available-for-sale financial assets.

For financial instruments which are not at fair value through profit or loss, transaction costs are included in the initial measurement of the instrument.

Transaction costs on financial instruments at fair value through profit or loss are recognised in profit or loss.

Subsequent measurement

Financial instruments at fair value through profit or loss are subsequently measured at fair value, with gains and losses arising from changes in fair value being included in profit or loss for the period.

Net gains or losses on the financial instruments at fair value through profit or loss include dividends and interest.

Loans and receivables are subsequently measured at amortised cost, using the effective interest method, less accumulated impairment losses.

Financial liabilities at amortised cost are subsequently measured at amortised cost, using the effective interest method.

Derecognition

Financial assets are derecognised when the rights to receive cash flows from the investments have expired or have been transferred and the Authority has transferred substantially all risks and rewards of ownership.

Trade and other receivables

Trade receivables are measured at initial recognition at fair value, and are subsequently measured at amortised cost using the effective interest rate method. Appropriate allowances for estimated irrecoverable amounts are recognised in profit or loss when there is objective evidence that the asset is impaired. Significant financial difficulties of the debtor, probability that the debtor will enter bankruptcy or financial reorganisation, and default or delinquency in payments (more than 30 days overdue) are considered indicators that the trade receivable is impaired. The allowance recognised is measured as the difference between the asset's carrying amount and the present value of estimated future cash flows discounted at the effective interest rate computed at initial recognition.

The carrying amount of the asset is reduced through the use of an allowance account, and the amount of the loss is recognised in profit or loss within operating expenses. When a trade receivable is uncollectable, it is written off against the allowance account for trade receivables. Subsequent recoveries of amounts previously written off are credited against operating expenses in profit or loss.

Trade and other receivables are classified as loans and receivables.

Trade and other payables

Trade payables are initially measured at fair value, and are subsequently measured at amortised cost, using the effective interest rate method.

Cash and cash equivalents

Cash and cash equivalents comprise cash on hand and demand deposits, and other short-term highly liquid investments that are readily convertible to a known amount of cash and are subject to an insignificant risk of changes in value. These are initially and subsequently recorded at fair value.

Cash and cash equivalents is classified as loans and receivables.

1.5 Leases

A lease is classified as a finance lease if it transfers substantially all the risks and rewards incidental to ownership. A lease is classified as an operating lease if it does not transfer substantially all the risks and rewards incidental to ownership.

Finance leases – lessee

Finance leases are recognised as assets and liabilities in the statement of financial position at amounts equal to the fair value of the leased property or, if lower, the present value of the minimum lease payments. The corresponding liability to the lessor is included in the statement of financial position as a finance lease obligation.

The lease payments are apportioned between the finance charge and reduction of the outstanding liability.The finance charge is allocated to each period during the lease term so as to produce a constant periodic rate on the remaining balance of the liability.

Operating leases – lessee

Operating lease payments are recognised as an expense on a straight-line basis over the lease term. The difference between the amounts recognised as an expense and the contractual payments are recognised as an operating lease asset. This liability is not discounted.

Any contingent rents are expensed in the period they are incurred.

1.6 Taxation

The Roads Authority is not liable for Income Tax nor Value Added Tax.

1.7 Impairment of assets

The Authority assesses at each end of the reporting period whether there is any indication that an asset may be impaired. If any such indication exists, the Authority estimates the recoverable amount of the asset.

Irrespective of whether there is any indication of impairment, the Authority also:
- tests intangible assets with an indefinite useful life or intangible assets not yet available for use for impairment annually by comparing its carrying amount with its recoverable amount. This impairment test is performed during the annual period and at the same time every period.
- tests goodwill acquired in a business combination for impairment annually.

If there is any indication that an asset may be impaired, the recoverable amount is estimated for the individual asset. If it is not possible to estimate the recoverable amount of the individual asset, the recoverable amount of the cash-generating unit to which the asset belongs is determined.

The recoverable amount of an asset or a cash-generating unit is the higher of its fair value less costs to sell and its value in use.

If the recoverable amount of an asset is less than its carrying amount, the carrying amount of the asset is reduced to its recoverable amount. That reduction is an impairment loss.

An impairment loss of assets carried at cost less any accumulated depreciation or amortisation is recognised immediately in profit or loss. Any impairment loss of a revalued asset is treated as a revaluation decrease.

An entity assesses at each reporting date whether there is any indication that an impairment loss recognised in prior periods for assets other than goodwill may no longer exist or may have decreased. If any such indication exists, the recoverable amounts of those assets are estimated.

The increased carrying amount of an asset other than goodwill attributable to a reversal of an impairment loss does not exceed the carrying amount that would have been determined had no impairment loss been recognised for the asset in prior periods.

A reversal of an impairment loss of assets carried at cost less accumulated depreciation or amortisation other than goodwill is recognised immediately in profit or loss. Any reversal of an impairment loss of a revalued asset is treated as a revaluation increase.

1.8 Employee benefits

Defined benefit plans
For defined benefit plans the cost of providing the benefits is determined using the projected unit credit method.

Actuarial valuations are conducted every second year by independent actuaries.

Consideration is given to any event that could impact the funds up to the end of the reporting period where the interim valuation is performed at an earlier date.

Past service costs are recognised immediately to the extent that the benefits are already vested, and are otherwise amortised on a straight line basis over the average period until the amended benefits become vested.

Actuarial gains and losses are recognised in the year in which they arise, in other comprehensive income.

When it is virtually certain that another party will reimburse some or all of the expenditure required to settle a defined benefit obligation, the right to reimbursement is recognised as a separate asset. The asset is measured at fair value. In all other respects, the asset is treated in the same way as plan assets. In profit or loss, the expense relating to a defined benefit plan is presented as the net of the amount recognised for a reimbursement.

The amount recognised in the statement of financial position represents the present value of the defined benefit obligation as adjusted for unrecognised actuarial gains and losses and unrecognised past service costs.

1.9 Provisions and contingencies

Provisions are recognised when:
- the Authority has a present obligation as a result of a past event;
- it is probable that an outflow of resources embodying economic benefits will be required to settle the obligation; and
- a reliable estimate can be made of the obligation.

The amount of a provision is the present value of the expenditure expected to be required to settle the obligation.

Provisions are not recognised for future operating losses.

If an entity has a contract that is onerous, the present obligation under the contract shall be recognised and measured as a provision.

1.10 Government grants

Government grants are recognised when there is reasonable assurance that:
- the Authority will comply with the conditions attaching to them; and
- the grants will be received.

Government grants are recognised as income over the periods necessary to match them with the related costs that they are intended to compensate.

A government grant that becomes receivable as compensation for expenses or losses already incurred or for the purpose of giving immediate financial support to the entity with no future related costs is recognised as income of the period in which it becomes receivable.

Government grants related to assets, including non-monetary grants at fair value, are presented in the statement of financial position by setting up the grant as deferred income.

Grants related to income are presented as a credit in the profit or loss (separately).

1.11 Deficit or surplus for the year

The Roads Authority is an entity created to manage Namibia's national road network and not for the purpose of generating profits. In order to perform its duties, funds are given to Roads Authority by the Road Fund Administration. At the end of the financial year the deficit or surplus is transferred from the Roads Authority to Road Fund Administration.

1.12 Other income

When inventories are sold, the carrying amount of those inventories is recognised as an expense in the period in which the related revenue is recognised. The amount of any write-down of inventories to net realisable value and all losses of inventories are recognised as an expense in the period the write-down or loss occurs. The amount of any reversal of any write-down of inventories, arising from an increase in net realisable value, is recognised as a reduction in the amount of inventories recognised as an expense in the period in which the reversal occurs.

The related cost of providing services recognised as revenue in the current period is included in cost of sales.

Contract costs comprise:
- costs that relate directly to the specific contract;
- costs that are attributable to contract activity in general and can be allocated to the contract; and
- such other costs as are specifically chargeable to the customer under the terms of the contract.

1.13 Borrowing costs

Borrowing costs that are directly attributable to the acquisition, construction or production of a qualifying asset are capitalised as part of the cost of that asset until such time as the asset is ready for its intended use. The amount of borrowing costs eligible for capitalisation is determined as follows:
- Actual borrowing costs on funds specifically borrowed for the purpose of obtaining a qualifying asset less any temporary investment of those borrowings.
- Weighted average of the borrowing costs applicable to the entity on funds generally borrowed for the purpose of obtaining a qualifying asset. The borrowing costs capitalised do not exceed the total borrowing costs incurred.

The capitalisation of borrowing costs commences when:
- expenditures for the asset have occurred;
- borrowing costs have been incurred, and
- activities that are necessary to prepare the asset for its intended use or sale are in progress.

Capitalisation is suspended during extended periods in which active development is interrupted.

Capitalisation ceases when substantially all the activities necessary to prepare the qualifying asset for its intended use or sale are complete.

All other borrowing costs are recognised as an expense in the period in which they are incurred.

2. NEW STANDARDS AND INTERPRETATIONS

2.1 Standards and interpretations effective and adopted in the current year

In the current year, the Authority has adopted the following standards and interpretations that are effective for the current financial year and that are relevant to its operations:

IAS 19 Employee Benefits Revised

- Require recognition of changes in the net defined benefit liability (asset) including immediate recognition of defined benefit cost, disaggregation of defined benefit cost into components, recognition of remeasurements in other comprehensive income, plan amendments, curtailments and settlements
- Introduce enhanced disclosures about defined benefit plans
- Modify accounting for termination benefits, including distinguishing benefits provided in exchange for service and benefits provided in exchange for the termination of employment and affect the recognition and measurement of termination benefits
- Clarification of miscellaneous issues, including the classification of employee benefits, current estimates of mortality rates, tax and administration costs and risk-sharing and conditional indexation features

The effective date of the amendment is for years beginning on or after 01 January 2013.

The Authority adopted the amendment for the first time in the 2014 financial statements.

The impact of the amendment is set out in note 3 Changes in Accounting Policy.

2.2 Standards and interpretations not yet effective

The Authority has chosen not to early adopt the following standards and interpretations, which have been published and are mandatory for the Authority's accounting periods beginning on or after 01 April 2014 or later periods:

IFRS 9 Financial Instruments

This new standard is the first phase of a three phase project to replace IAS 39 Financial Instruments: Recognition and Measurement. To date, the standard includes chapters for classification, measurement and derecognition of financial assets and liabilities. The following are main changes from IAS 39:

- Financial assets will be categorised as those subsequently measured at fair value or at amortised cost.
- Financial assets at amortised cost are those financial assets where the business model for managing the assets is to hold the assets to collect contractual cash flows (where the contractual cash flows represent payments of principal and interest only). All other financial assets are to be subsequently measured at fair value.
- Under certain circumstances, financial assets may be designated as at fair value.
- For hybrid contracts, where the host contract is an asset within the scope of IFRS 9, then the whole instrument is classified in accordance with IFRS 9, without separation of the embedded derivative. In other circumstances, the provisions of IAS 39 still apply.
- Voluntary reclassification of financial assets is prohibited. Financial assets shall be reclassified if the entity changes its business model for the management of financial assets. In such circumstances, reclassification takes place prospectively from the beginning of the first reporting period after the date of change of the business model.
- Financial liabilities shall not be reclassified.
- Investments in equity instruments may be measured at fair value through other comprehensive income. When such an election is made, it may not subsequently be revoked, and gains or losses accumulated in equity are not recycled to profit or loss on derecognition of the investment. The election may be made per individual investment.
- IFRS 9 does not allow for investments in equity instruments to be measured at cost.
- The classification categories for financial liabilities remains unchanged. However, where a financial liability is designated as at fair value through profit or loss, the change in fair value attributable to changes in the liabilities credit risk shall be presented in other comprehensive income. This excludes situations where such presentation will create or enlarge an accounting mismatch, in which case, the full fair value adjustment shall be recognised in profit or loss.

Offsetting Financial Assets and Financial Liabilities (Amendments to IAS 32)

Clarification of certain aspects concerning the requirements for offsetting financial assets and financial liabilities.

The effective date of the amendment is for years beginning on or after 01 January 2014.

The Authority expects to adopt the amendment for the first time in the 2015 financial statements.

It is unlikely that the amendment will have a material impact on the Authority's financial statements.

New standard setting out guidance on the measurement and disclosure of items measured at fair value or required to be disclosed at fair value in terms of other IFRS's.

The effective date of the standard is for years beginning on or after 01 January 2013.

The Authority expects to adopt the standard for the first time in the 2014 financial statements.

3. CHANGES IN ACCOUNTING POLICY

The financial statements have been prepared in accordance with International Financial Reporting Standards on a basis consistent with the prior year except for the adoption of the following new or revised standards.

IAS 19 Employee benefits

During the year, the Authority changed its accounting policy with respect to the treatment of unrecognised gains and losses of Post retirement benefit obligations. In order to conform with the benchmark treatment in of IAS 19 - Employee benefit revised. The Authority now recognises all actuarial gains and losses of the Post retirement benefit obligations,in other comprehensive income.

The aggregate effect of the changes in accounting policy on the financial statements is as follows:

	2014 N$ '000	2013 N$ '000	2012 N$ '000
Statement of Financial Position			
Post retirement obligation			
Previously stated	-	62 783	54 819
Adjustment	-	(385)	6 057
	-	**62 398**	**60 876**
Trade and other receivables			
Previously stated	-	220 065	243 747
Adjustment	-	(385)	6 057
	-	**219 680**	**249 804**
Statement of Comprehensive income			
Other comprehensive income			
Adjustment	-	(7 171)	-

4. PROPERTY, PLANT AND EQUIPMENT

	2014 Cost / Valuation	Accumulated depreciation	Carrying value	2013 Cost / Valuation	Accumulated depreciation	Carrying value	2012 Cost / Valuation	Accumulated depreciation	Carrying value
Land and buildings	29 614	(2 995)	26 619	29 614	(2 498)	27 116	29 585	(2 000)	27 585
Leased vehicles	23 455	(15 274)	8 181	22 718	(13 319)	9 399	18 869	(10 979)	7 890
Plant and machinery	7 555	(4 563)	2 992	5 613	(3 897)	1 716	4 149	(3 534)	615
Furniture and fixtures	11 136	(6 905)	4 231	10 344	(6 015)	4 329	9 044	(5 330)	3 714
Motor vehicles	1 457	(1 201)	256	1 457	(1 201)	256	1 457	(1 091)	366
Computer equipment	27 207	(22 482)	4 725	24 270	(19 281)	4 989	21 762	(16 606)	5 156
Work in progress									
- Head Office	11 731	-	11 731	-	-	-	-	-	-
Total	**112 155**	**(53 420)**	**58 735**	**94 016**	**(46 211)**	**47 805**	**84 866**	**(39 540)**	**45 326**

RECONCILIATION OF PROPERTY, PLANT AND EQUIPMENT - 2014

	Opening balance	Additions	Depreciation	Total
Buildings	27 116	-	(497)	26 619
Leased vehicles	9 399	2 580	(3 798)	8 181
Plant and machinery	1 716	1 942	(666)	2 992
Furniture and fixtures	4 329	792	(890)	4 231
Motor vehicles	256	-	-	256
Computer equipment	4 989	2 937	(3 201)	4 725
Work In Progress-Head Office	-	11 731	-	11 731
	47 805	**19 982**	**(9 052)**	**58 735**

RECONCILIATION OF PROPERTY, PLANT AND EQUIPMENT - 2013

	Opening balance	Additions	Returned	Depreciation	Total
Buildings	27 585	29	-	(498)	27 116
Leased vehicles	7 890	5 432	(234)	(3 689)	9 399
Plant and machinery	615	1 464	-	(363)	1 716
Furniture and fixtures	3 714	1 300	-	(685)	4 329
Motor vehicles	366	-	-	(110)	256
Computer equipment	5 156	2 508	-	(2 675)	4 989
	45 326	**10 733**	**(234)**	**(8 020)**	**47 805**

SHOW CASH FLOWS OF PROPERTY, PLANT AND EQUIPMENT ADDITIONS.

	2014 N$ ‘000	2013 N$ ‘000
Property,plant and equipment additions (per above note)	19 982	10 733
Less: Additions financed by Finance Lease	(2 580)	(5 432)
Additions per Statement of Cash Flows	**17 402**	**5 301**

Other information

List of property, plant and equipment are available for inspection at the registered office.

5. INTANGIBLE ASSETS

	2014 Cost / Valuation	Accumulated amortisation	Carrying value	2013 Cost / Valuation	Accumulated amortisation	Carrying value	2012 Cost / Valuation	Accumulated amortisation	Carrying value
Computer software	**11 058**	**(5 826)**	**5 232**	**5 829**	**(5 664)**	**165**	**5 831**	**(5 493)**	**338**

RECONCILIATION OF INTANGIBLE ASSETS - 2014

	Opening balance	Additions	Amortisation	Total
Computer software	**165**	**5 229**	**(162)**	**5 232**

RECONCILIATION OF INTANGIBLE ASSETS - 2013

	Opening balance	Amortisation	Total
Computer software	**338**	**(173)**	**165**

RECONCILIATION OF INTANGIBLE ASSETS - 2012

	Opening balance	Amortisation	Total
Computer software	**509**	**(171)**	**338**

6. RETIREMENT BENEFITS

DEFINED BENEFIT PLAN

The Authority provides post-retirement medical benefits to retired staff members under certain conditions. The Defined Plan is unfunded.

	2014 N$ '000	2013 N$ '000	2012 N$ '000
Carrying value			
Present value of the defined benefit obligation-wholly unfunded	(62 398)	(60 876)	(52 008)
Net expenses recognised in the statement of comprehensive income	(8 624)	(8 693)	(8 868)
Actuarial gain	-	7 171	-
	(71 022)	**(62 398)**	**(60 876)**
Reconciliation of the Retirement benefit obligation			
Opening balance	62 398	60 876	52 008
Current service cost	4 801	4 882	4 963
Interest cost	4 886	4 776	4 665
Benefits paid	(1 063)	(965)	(760)
Actuarial loss / (Gain)	-	(7 171)	-
	71 022	**62 398**	**60 876**
Net expense recognised in Statement of Comprehensive Income			
Current service cost	4 801	4 882	4 963
Interest cost	4 886	4 776	4 665
Benefits paid	(1 063)	(965)	(760)
	8 624	**8 693**	**8 868**
Key assumptions used			
Assumptions used on last valuation date.			
Discount rates	7,50 %	7,50 %	8,97 %
Medical Aid Contribution inflation	6,50 %	6,50 %	7,89 %

7. LONG-TERM PREPAYMENTS

Represents advance payments of 5% of the tender amount awarded to the Roads Contractor Company Ltd , Quiver Tree Investments One Three CC and Jacomina Johanna Burger T/As JJ Blading CC. The contract is for a period of five years (60 months), the repayment will be deducted from amounts due to the contractor in six equal installments, starting six months before the contract completion date.

	2014 N$ '000	2013 N$ '000	2012 N$ '000
Contractor			
Road Contractor Company Ltd (RCC)	21 725	21 725	-
Quiver Tree Investments One Three CC	10 188	8 052	-
Jacomina Johanna Burger T/As JJ Blading CC	1 655	-	-
	33 568	**29 777**	**-**

8. TRADE AND OTHER RECEIVABLES

	2014 N$ '000	2013 N$ '000	2012 N$ '000
Road Fund Administration	200 362	169 695	187 376
Other debtors	710	399	625
Prepayments to Contractors	54 705	49 586	61 803
	255 777	**219 680**	**249 804**

The Road Fund Administration and other receivables are non interest bearing.

Trade and other receivables past due but not impaired

The ageing of amounts past due but not impaired is as follows:

Neither past due nor impaired	246 753	219 680	249 804

9. CASH AND CASH EQUIVALENTS

	2014 N$ '000	2013 N$ '000	2012 N$ '000
Cash and cash equivalents consist of:			
Cash on hand	16	16	34
Bank balances	43 393	59 889	7 730
	43 409	**59 905**	**7 764**

Credit quality of cash at bank and short term deposits, excluding cash on hand

The credit quality of cash at bank and short term deposits, excluding cash on hand that are neither past due nor impaired can be assessed by reference to external credit ratings or historical information about counterparty default rates:

Credit rating			
Bank Windhoek Limited (A1+ Moody's credit rating)	**43 393**	**59 889**	**7 730**

10. GOVERNMENT CONTRIBUTION

When the Authority was incorporated the Government of Namibia made a contribution in the form of land.

	2014 N$ '000	2013 N$ '000	2012 N$ '000
Transfer of immovable properties	8 992	8 992	8 992

11. FINANCE LEASE OBLIGATION

	2014 N$ '000	2013 N$ '000	2012 N$ '000
Minimum lease payments due			
- within one year	3 762	3 359	3 127
- in second to fifth year inclusive	4 959	6 678	5 430
Present value of minimum lease payments	**8 721**	**10 037**	**8 557**
Non-current liabilities	4 959	6 678	5 430
Current liabilities	3 762	3 359	3 127
	8 721	**10 037**	**8 557**

The Authority is leasing certain motor vehicles under finance leases from Avis Fleet Services Namibia, a division of Zeda Namibia (Pty) Ltd.

The lease terms ranges between 36 months to 86 months, of which the majority of the vehicles are leased over a 5 year period. The fixed monthly installments included 44% for maintenance which have been excluded from the calculation of the lease liability.

Interest rates are linked to prime at the contract date. All leases have fixed repayments and no arrangements have been entered into for contingent rent.

12. DEFERRED INCOME

	PIARC N$ 000	MWT N$ 000	RFA N$ 000	RA BOOK N$ 000	Total N$ 000
Balance at 31 March 2012	351	2 169	86 290	175	88 985
Received during the year	-	-	54 111	9	54 120
Released to the statement of comprehensive income	(47)	-	(35 488)	-	-
Balance at 31 March 2013	**304**	**2 169**	**104 913**	**184**	**107 570**
Received during the year	-	-	50 397	9	50 406
Released to the statement of comprehensive income	-	-	(26 505)	-	(26 505)
Balance at 31 March 2014	**304**	**2 169**	**128 805**	**193**	**131 471**

Deferred revenue relates to grants in the form of assets received and advances due to the Roads Authority for materials on site,

Deferred revenue is released to the statement of comprehensive income in equal annual amounts over the expected useful life of the asset or when the goods are delivered.

MWT represents the Ministry of Works and Transport and RFA represents Road Fund Administration. The RA History Book project is undertaken by the Roads Authority to write a book on the history of the Namibian road network. PIARC represents the Permanent International Association of Road Congresses (World Road association).

13. PROVISIONS

RECONCILIATION OF PROVISIONS - 2014

	Opening balance	Additions	Utilised during the year	Leave encashment	Total
Provision for bonus	1 636	1 803	(1 636)	-	1 803
Provision for leave	13 024	3 650	-	(770)	15 904
Other provisions	-	1 838	-	-	1 838
	14 660	**7 291**	**(1 636)**	**(770)**	**19 545**

RECONCILIATION OF PROVISIONS - 2013

	Opening balance	Additions	Utilised during the year	Leave encashment	Total
Provision for bonus	1 516	1 636	(1 516)	-	1 636
Provision for leave	10 792	9 629	(6 760)	(637)	13 024
	12 308	**11 265**	**(8 276)**	**(637)**	**14 660**

RECONCILIATION OF PROVISIONS - 2012

	Opening balance	Additions	Utilised during the year	Leave encashment	Total
Provision for bonus	1 506	3 132	(3 122)	-	1 516
Provision for leave	9 335	9 102	(6 959)	(686)	10 792
Provision - Trade related	269	-	(269)	-	-
	11 110	**12 234**	**(10 350)**	**(686)**	**12 308**

The provisions are not discounted to present value as they are deemed that the time value of money will not be material. The impact of staff on the provisions is not deemed to be material; accordingly the impact of this has not been calculated and recorded.

Leave pay
Leave pay provision is based on actual leave days due to employees and is calculated using current remuneration packages. During the year N$ 2 880 680 (2013: 2 868 000; 2012: 2 143 000) was charged to the statement of comprehensive income.

Annual Bonus
The annual bonus is normally paid out in November. The bonus is accrued monthly on a time proportion basis for all employees in employment at year end. The provision is based on current remuneration packages. During the year N$ 145 672 000 (2013: N$ 143 000 000 2012: N$ 62 000 000) was charged to the statement of comprehensive income.

Other provisions
This provision relates to IT equipment which were held by customs at year end. Invoice were not yet obtained from supplier. The Authority made a provision for the equipment which were purchased before year end.

14. TRADE AND OTHER PAYABLES

	2014 N$ '000	2013 N$ '000	2012 N$ '000
Trade payables	152 651	149 236	118 218
Retention account	1 633	1 753	2 610
	154 284	**150 989**	**120 828**

15. REVENUE

	2014 N$ '000	2013 N$ '000	2012 N$ '000
Grants received: Road Fund Administration	1 406 526	1 247 440	1 006 550
Interest received	1 826	1 363	1 814
	1 408 352	**1 248 803**	**1 008 364**

16. ADMINISTRATIVE EXPENDITURE

	2014 N$ '000	2013 N$ '000	2012 N$ '000
Auditors' remuneration			
Audit fees	320	304	392
Professional fees			
Consulting fees	2 159	1 160	3 246
Depreciation			
- Buildings	498	498	498
- Computer equipment	3 201	2 675	2 117
- Furniture and equipment	666	685	458
- Motor vehicles	-	110	108
- Plant and equipment	890	363	184
- Leased motor vehicle	3 798	3 689	3 484
	9 053	8 020	6 849
Amortisation - Computer software	161	171	171
Operating lease expenses			
- Computer equipment	-	154	100
- Premises	3 646	3 418	2 933
- Motor vehicles	5 120	4 457	3 130
	8 766	8 029	6 163
Repairs and maintenance	941	1 461	1 180
Transport	9 737	8 280	8 252
Utilities	7 770	11 619	10 014
Subscriptions and membership fees	670	251	485
Furniture and office equipment	962	814	748
Other expenses	1 028	678	223
- Advertising	3 739	3 538	3 091
- Cleaning and consumables	2 999	2 993	2 730
- Courier and postage	510	381	351
- Insurance	1 845	1 559	1 967
- Legal fees	1 413	600	517
- Printing and stationery	1 861	1 662	1 575
- Specialized Service	11 477	4 069	2 951
- Subsistence allowance and Travelling expenses	9 404	7 642	8 322
- Training	5 003	4 628	3 762
	59 359	50 175	46 168
Total	79 818	67 859	61 353
Employee Costs			
Salaries	127 822	111 561	101 738
Social Security Costs	407	278	253
Bonuses	3 790	3 354	3 059
Pension Costs	5 284	4 902	4 594
Post-employment benefit other than pension	8 624	15 136	9 738
Bursaries	2 007	1 260	1 225
Directors emoluments	485	547	585
Medical aid contributions	9 952	8 742	7 659
Training costs	2 213	1 702	1 623
	160 584	147 481	130 476

17. ROAD MANAGEMENT QUALIFICATION

	2014 N$ '000	2013 N$ '000	2012 N$ '000
Polytechnic of Namibia	78	227	8 497

Roads Authority has entered into a collaborative agreement with the Polytechnic of Namibia for the development and implementation of an academic programme in Road Transport Management beyond the Certificate level. This qualification is envisaged to improve the safety and efficiency standards, not only of the national but also of the SADC - regional road sectors through the steady supply of well trained professionals. It may become further impetus for the establishment of a SADC Institute of Roads Transport Management. Roads Authority has undertook to bear the costs for the curriculum, syllabi and the teaching, learning and assessment materials, as well as those of a Coordinator and all the costs related to the branding of the programme.

18. FINANCE COSTS

	2014 N$ '000	2013 N$ '000	2012 N$ '000
Finance leases	909	1 022	952

19. CASH (USED IN) GENERATED FROM OPERATIONS

	2014 N$ '000	2013 N$ '000	2012 N$ '000
Surplus after transfers	-	-	-
Adjustments for:			
Finance costs	909	1 022	952
Depreciation	9 052	8 020	6 854
Amortisation	161	171	171
Provision for post retirement medical aid obligation	8 624	7 964	9 738
Increase in Provision	4 885	2 352	1 197
Release from deferred revenue	(26 505)	(35 534)	(3 384)
Transfer to deferred revenue-grants received	50 406	54 120	64 795
Interest received	(1 826)	(1 363)	(1 814)
Changes in working capital:			
Trade and other receivables	(36 097)	23 682	(97 038)
Trade and other payables	3 295	30 163	24 285
	12 904	90 597	5 756

20. COMMITMENTS

	2014 N$ '000	2013 N$ '000	2012 N$ '000
Road network commitments			
• Contracted for	394 862	1 337 405	1 313 466
• Not contracted for	1 256 992	1 485 505	1 986 277

This committed expenditure relates to the construction and rehabilitation of the National roads in Namibia and will be financed by grants and direct disbursements.

Operating leases

Minimum lease payments due			
- within one year	3 790	3 664	3 292
- in second to fifth year inclusive	4 356	13 342	13 320
- later than five years	**8 146**	**17 006**	**16 612**

Operating lease payments represent rentals payable by the Authority for certain of its office properties.

21. RELATED PARTIES

Relationships
Related party relationship exists between the Authority and:

Key Management	
Roads Contractor Company	State Owned Entity
Road Fund Administration	State Owned Entity

Related party balances

Related Parties

	2014 N$ '000	2013 N$ '000	
Amount receivable at the year end	200 362	169 695	
Deferred Revenue	(128 806)	(104 914)	
Roads Contractor Company	(12 820)	(36 400)	-
Related party transactions			
Grants received during the year	1 398 475	1 247 440	-
Transfer of comprehensive income	(5 159)	(11 257)	-
Roads Contractor Company	208 561	344 438	-
Key management			
Key management remuneration	8 391	7 426	

Key Management constitutes of the Chief Executive Officer (CEO) and seven (7) divisional managers

22. RISK MANAGEMENT

Capital risk management

The Roads Authority is an agent of the Government reporting to the Ministry of Works and Transport and manages the national road network. The Roads Authority came into being on 1 April 2000. The Ministry of Works and Transport, in consultation with the Ministry of Finance, and on such conditions as the Ministry may determine, have transferred to the Roads Authority, with effect from 1 April 2000, such assets, liabilities, rights or obligations of the State which relate to or connected with the management of roads by the Ministry as may, in the opinion of the Minister, be required by the Authority.

The afore-mentioned is the way in which the entity manages its capital structure and makes amendments to it in light of changes in the economic conditions. The Ministry of Works and Transport, in consultation with the Ministry of Finance, may decide to adjust or maintain the capital structure using government contributions as a channel of funding.

Financial risk management

The Authority's principal financial liabilities comprise of trade payables and retention's for various projects and routine maintenance performed by the Authority. The main purpose of these financial liabilities is to maintain adequate cashflows for the entity, to be able to continue operations. The Authority has various financial assets such as trade receivables, cash and short term deposits, which arise directly from its operations.

The main purpose for the large trade debtors is to enable the Authority to finance its operations. The main risks arising from the entity's financial instruments are liquidity and credit risk. The Authority is not subject to interest rate risk as it does not hold any loans or fixed borrowings from financial institutions. The interest received on cash and cash equivalents at financial institutions are minimal and therefore interest rate risk has been identified as not significant. It is also not subject to foreign exchange risk as the entity has no transactions denominated in a foreign currency. It also does not hold foreign interests. Management reviews and agrees policies for managing each of these risks which are summarised below.

Liquidity risk

The Authority's risk to liquidity is a result of the funds available to cover future commitments. The Authority manages liquidity risk through an ongoing review of future commitments and support from the Road Fund Administration in the form of funding. Cashflow forecasts are prepared and adequate funding facilities are monitored.

The table below summarises the maturity profile of the entity's financial liabilities at 31 March 2014 based on contractual undiscounted payments.

	On demand N$ '000	Less than 12 months N$ '000	More than 12 months N$ '000	Total N$ '000
At 31 March 2014				
Trade and other payables	154 282	-	-	154 282
Finance lease obligation	-	3 762	4 959	8 722
At 31 March 2013				
Trade and other payables	150 985	-	-	150 985
Finance lease obligation	-	3 359	6 678	10 037

Financial instruments by category

	Loans and receivables N$ '000	Financial liabilities at amortised cost N$ '000
At 31 March 2014		
Financial assets		
• Trade and other receivables	247 153	-
• Cash and cash equivalents	43 409	-
Financial liabilities		
• Trade and other payables	-	154 282
• Finance lease obligations	-	8 722

	Loans and receivables N$ '000	Financial liabilities at amortised cost N$ '000	Total N$ '000
At 31 March 2013			
Financial assets			
• Trade and other receivables	219 680	-	220 065
• Cash and cash equivalents	59 905	-	59 905
Financial liabilities			
• Trade and other payables	-	150 988	150 988
• Finance lease obligations	-	10 037	10 037

Interest rate risk

The Authority's interest rate risk arises from long-term finance lease obligation. Borrowings issued at variable rates expose the Authority to cash flow interest rate risk. Borrowings issued at fixed rates expose the Authority to fair value interest rate risk. The Authority is not significantly exposed to cash flow interest rate risk.

Credit risk

Credit risk consists mainly of cash deposits, cash equivalents and trade debtors. The Authority only deposits cash with major banks with high quality credit standing and limits exposure to any one counter-party.

Financial assets exposed to credit risk at year end were as follows:

	2014 N$ '000	2013 N$ '000	2012 N$ '000
Financial instrument			
Finance lease obligation	8 722	10 037	8 557

Fair values

Set out below is a comparison by category of carrying amount and fair values of all of the Authority's financial instruments.

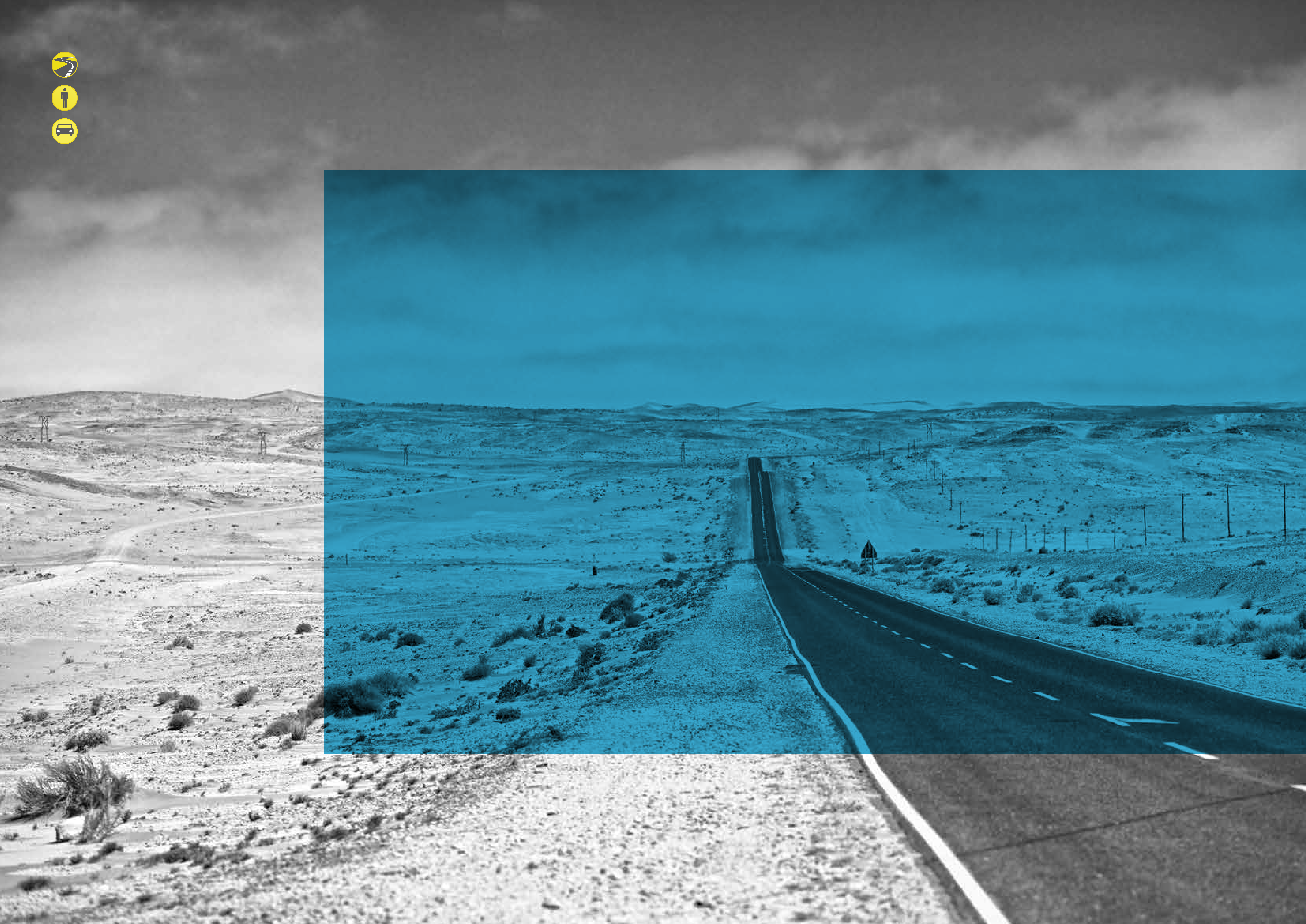
	Carrying amounts			Fair values		
	2014	2013	2012	2014	2013	2012
Financial instrument						
Trade and other receivables	247 153	219 680	249 804	247 153	219 680	249 804
Cash and cash equivalents	43 409	59 905	7 764	43 409	59 905	7 764
	290 562	**279 585**	**257 568**	**290 562**	**279 585**	**257 568**
Trade and other payables	154 282	150 985	120 822	154 282	150 985	120 822
Finance lease obligation	8 722	10 037	8 557	8 722	10 037	8 557
	163 004	**161 022**	**129 379**	**163 004**	**161 022**	**129 379**

SUPPLEMENTARY SCHEDULE NOT FORMING PART OF THE ANNUAL FINANCIAL STATEMENTS AS AT 31 MARCH 2014

CONSTRUCTION AND REHABILITATION PROJECTS

Construction and Rehabilitation of Roads

Proj Nr	IBMS Project Code	Project Name	Status	Total Estimated Cost	Expenditure Up To 31 March 2013	RA Expenditure 2013-2014	Direct Disbursements 2013-2014	Funding Source	Expenditure Up To 31 March 2014
0023	CPHF22R0100029	TR 1/6: Windhoek - Okahandja Rehabilitation	On-going, Completion Expected August 2016	307,112,812.93	5,039,682.00	9,429,051.51		RFA	14,468,733.51
0032	CPWD00DU100001	MR 67: Kamanjab - Omakange	Phase 1: complete	173,697,140.90	353,871,474.00			ADB	353,871,474.00
0044	CPWD00DU100002		Phase 2: complete	180,174,333.40				GRN	
0034	CPWD00R0100001	TR 7/1: Okahandja - Karibib	Phase 1: complete	346,418,384.00	345,841,114.00			GRN	345,841,114.00
	CPHF22R0100005		Phase 2: Complete	126,331,849.00	115,179,471.25	219,811.08	2,504,917.85	RFA	117,904,200.18
0035	CLWD00DN200002	DR 3644: Ompundja - Eheke LBC	Complete	24,906,958.00	24,906,958.00			RFA	24,906,958.00
								KFW	
0038	CPWD00DU100003	MR 110: Rundu - Elundu	Phase 1: complete	289,830,408.00	289,830,408.00			JBIC	289,830,408.00
				79,651,361.00	78,901,361.00			GRN	78,901,361.00
0062	CPWD00DU100004		Phase 2: complete	568,159,779.00	568,159,779.00			JBIC	568,159,779.00
				510,927,886.40	510,819,758.00		108,128.40	GRN	510,927,886.40
0039	CPWD00DU100005	MR 120: Okatana - Endola - Onunho	On-going. Completion Expected November 2014	194,319,250.48	59,908,093.00	101,179,912.97		RFA	161,088,005.97
0045	CPWD00DU100006	MR 122: Okahao - Omakange (Prev 4)	Complete	377,831,376.66	376,269,201.31		1,562,175.35	GRN	377,831,376.66
0058	CLWD00DN200003	DR 3502: Kongola - Zambia Borders LBC	Complete	20,274,356.00	20,274,356.00			GRN	20,274,356.00
0059	CLWD00DN200004	DR 3507: Ngoma - Muyako LBC	Complete	32,998,402.11	32,998,402.00			GRN	32,998,402.00
0061	CLWD00DN200006	Drainage Structures in Kunene Region:	Complete	15,653,869.30	14,610,657.00	418,731.96		RFA	15,029,388.96
		DR3700 Ombuku river at Kongwati -Kunene	complete						
		Skelm river near Sesfontein Kunene Region	complete						
		MR128 Hoanib river near Khowarib	complete						
		DR2306 river crossing Omaruru at Okombahe	Construction complete						
0063	CPWD00DU100009	TR 14/2: Gobabis - Drimiopsis - Otjinene, Phase 1	Construction complete	409,901,340.05	380,584,373.00		29,316,967.05	GRN	409,901,340.05
0064	CPWD00DU200010	Divundu Bridge Widening	Complete	22,595,737.96	22,548,945.00	46,792.96		RFA	22,595,737.96
0069	CLWD00DN200007	DR 3642: Okahao - Outapi	Construction complete	56,789,282.47	52,279,206.00	(30,400.63)		RFA	52,248,805.37
								KfW	
0072	CLWD00DN200009	DR 3643: Ekamba - Onkani	Complete	66,023,726.00	66,023,726.00			RFA	66,023,726.00
								KfW	
0065	CLWD00DN200040	DR 3427: Tondoro - Kamupupu	Complete	13,740,929.00	13,740,929.00			GRN	13,740,929.00
0066	CLWD00DN200041	DR 3428: Rupara - Muveve/Ngcangcu	Complete	12,444,422.00	12,444,422.00			GRN	12,444,422.00
0067	CLWD00DN200039	DR 3660: MR92 - Omagongati	Complete	17,647,101.00	17,647,101.00			GRN	17,647,101.00
								KfW	
0068	CLWD00DN200042	DR 3610: Mangetti West Phase 1	Complete	42,158,415.00	42,158,415.00			GRN	42,158,415.00
	CPWD00DU100007	DR 3611: Oshikuku - Okalongo + extension	100% design complete	94,000,000.00	81,461,152.00			GRN	81,461,152.00
	CPHF22DU100001	TR 15/1: Tsumeb - Katwitwi (Section A, B & C)	100% construction complete	1,111,924,730.35	955,190,587.00		156,734,143.35	GRN	1,111,924,730.35
	CPHF06DU000003	DR 3603: Onayena - Okankolo	Ongoing, Construction Expected to be Completed Feb 2015	106,868,130.65	1,422,809.00			RFA	1,422,809.00
	CPHF00DU100232	DR 3615: Onamutuku - Oshikuku	Ongoing, Construction Expected to be Completed July 2014	81,517,059.31	1,044,490.00	21,928,362.01		RFA	22,972,852.01
	CPHF22DU100012	DR 212 Rosh Pinah & Sendelingsdrif	Complete	8,298,554.44	8,298,554.00			RFA/ADB	8,298,554.00
	CLHF00DN200290	DR 3524: Ngoma (Izimwe) - Nakabolelwa	50% Construction complete	50,585,519.65	15,132,459.00		12,157,752.50	GRN	27,290,211.50
	CLHF22DN200324	DR 3653: Oshikuku - Ekangolinene	Complete	41,713,235.55	34,547,556.00		3,790,102.53	GRN	38,337,658.53
	CLHF22DN200324	DR 3672: Outapi - Okapalelona	Complete	17,267,527.07	22,201,494.00		8,633,375.77	GRN	30,834,869.77
				13,567,342.70				KfW	
	CLHF22DN200323	DR 3671: Onamutai - Ongwediva - Okatana	Complete	29,860,816.31	35,678,156.00		17,644,730.26	GRN	53,322,886.26
				23,462,069.95				KfW	
	CPHF22DU100013	MR125: Liselo - Linyanti - Kongola - Singalamwe	Phase 1: 35% construction complete Phase 2: 15% construction complete	866,442,551.75	160,108,776.00		301,195,266.94	GRN	461,304,042.94
	CLHF00DN200291	DR 3427: Kamupupu - Mbururu	Ongoing, Construction Expected to be Completed August 2014	28,982,605.16	10,172,730.00		9,979,949.67	GRN	20,152,679.67
	CPHF22R0200014	Okandjengedi Bridge	100% construction complete	39,906,414.42	18,184,022.00	21,722,392.42		RFA/GRN	39,906,414.42
	CPHF22R0200010	Ongwediva Bridge	100% construction complete	54,663,550.01	30,141,879.00	24,521,671.01		RFA/GRN	54,663,550.01
	CLHF00DN200292	DR 3448: Kaisosi - Cuma	100% construction complete	52,492,524.92	42,811,605.00		9,680,919.92	GRN	52,492,524.92
	CLHF00DN200293	DR 3449: Tjova - Divayi	99% Construction complete	39,371,826.63	28,389,677.00		10,799,559.47	GRN	39,189,236.47
	CLHF22DN200005	DR 3673: Omuthiya - Onanke	50% construction complete	44,801,459.99	11,785,917.00		9,653,314.26	GRN	21,439,231.26
	CLHF22DN200008	DR 3657: Oshapapa (Oshali) - Epumbu	Ongoing, Construction Expected to be Completed June 2014	47,775,824.02	4,414,667.53		34,965,769.22	GRN	39,380,436.75
	CLHF22DN200004	DR 3649: Onalulago - Epembe	Ongoing, Construction Expected to be Completed February 2015	59,661,901.28	1,336,086.00		20,714,592.21	GRN	22,050,678.21
	CPHF00D0000050	Updating and Revising Roads Authority Manuals	70% complete	380,000.00	247,189.00			RFA	247,189.00
	CPHF19R0200001	Bridge 157 Rehoboth - Mariental	Ongoing, Construction Expected to be Completed October 2015	30,443,366.38	1,027,382.00	6,951,458.21		RFA	7,978,840.21
	CPHF19R0200010	Bridge 2312 Rehoboth - Mariental	Ongoing, Construction Expected to be Completed October 2016	10,147,788.79	342,461.00	6,054,734.75		RFA	6,397,195.75
	CPHF19R0200048	Bridge 158 Rehoboth - Mariental	Ongoing, Construction Expected to be Completed October 2017	10,147,788.79	342,461.00	2,317,152.73		RFA	2,659,613.73
	CLHF22DN200007	DR 3670: Oshandi - Eembahu	44% Construction Complete	39,616,714.38	4,420,134.00		35,196,580.38	GRN	39,616,714.38
				39,616,714.38	23,963.00			KfW	23,963.00
	CLHF22DN200016	DR 3670: Eembahu - Oshiweda		25,715,131.12	16,375.00			GRN	16,375.00
				20,204,745.88	109,167.00			KfW	109,167.00
	CPHF19DN000057	Three Bridges Linking Okahandja to Ovitoto	100% Complete	78,446,234.25	21,188,198.00		52,288,586.95	GRN	73,476,784.95
	CPHF19DU100056	MR 67: Omakange - Ruacana	17% Construction complete	517,329,195.00	10,650,476.00		20,242,987.88	GRN	30,893,463.88
	CPHF19DU100055	DR 3608: Omafo - Ongenga - Outapi	21% Construction complete	781,831,355.70	17,983,727.00		29,741,304.21	GRN	47,725,031.21
	CLHF19DN200022	DR 3603: Onayena - Onankali (DR 3674: Onayena - Omahenge)	33% Construction complete	50,106,015.07	1,503,153.00		28,088,178.51	GRN	29,591,331.51
	CPHF22DN300001	Gobabis Weighbridge	100% Construction complete	23,108,857.40	19,977,703.00	3,131,154.40		RFA	23,108,857.40
	CLHF19D0000024	Socio-economic Impact Study on three LB-Projects	50% complete	2,502,493.15	48,962.00			KFW	48,962.00
	CPHF19DU100065	Specialised Consultancy Service	on-going	98,980.00	98,980.00			GRN	98,980.00
	CPHF19DU100066	Elundu-Eenhana	100% Design complete	1,825,870.16	1,530,614.00	295,256.16		RFA	1,825,870.16
	CPHF19DU100018	MR 121Oshigambo-Eenanha	2% Construction complete	206,772,178.38	958,089.00		6,199,909.85	GRN	7,157,998.85
	CLHF19DN200026	Amwaanda-omutambo-omaove	100% Design complete	11,008,226.60	2,128,745.00		573,169.96	GRN	2,701,914.96
		Epato-Onkani	100% Design Complete	3,747,513.00			147,750.82	GRN	147,750.82
	CPHF19DU100001	Oshakati - Omungwelumwe-Ongenga	100% Design Complete	11,920,926.40			4,118,669.80	GRN	4,118,669.80
		DR:3688 Epako-Omuvelo Wakasamane	Ongoing, Construction Expected to be Completed November 2014	4,512,783.47			2,718,008.20	GRN	2,718,008.20
	CPHF22DU100069	MR91: Gobabis-Aminus-Aranos	Ongoing, Construction Expected to be Completed November 2018	591,766,526.03			8,348,517.39	GRN	8,348,517.39
	CPHF22DU100070	MR118 Roshpinah-Orandjemund	Ongoing, Construction Expected to be Completed November 2016	625,617,218.96			64,779,263.99	GRN	64,779,263.99
	CLHF22DN200030	DR 3508: ISIZE-SIFUHA-MALINDI-SCHUCKMANNBURG	85% Design Complete	10,926,175.55			2,297,900.82	GRN	2,297,900.82
	CPHF06DU000003	DR 3603 Onayena-Onkakolo	3% Construction Complete	106,868,130.65			2,812,326.56	GRN	2,812,326.56
		Etomba-Omundaungilo	20% Design Complete	7,651,618.07	112,500.00			RFA	112,500.00
		TOTAL		9,915,065,312.38	4,949,070,727.09	198,186,081.54	886,994,820.07		6,034,251,628.70

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